



HODGSON
PLANNING CONSULTANTS

Application for Land Use Consent

Paihia Terraces Ltd, 30 to 34 Selwyn Road, Paihia

HPC Reference PT2419

December 2024

Applicant	Paihia Terraces Ltd
Location	30 to 34 Selwyn Road, Paihia
Legal Description	Lots 1, 2 and 3 DP 328135 (RT NA114614 to NA114616)
Total Site Area	2280m ²
Territorial Authority	Far North District Council

Operative Far North District Plan, 2009

Zone	Paihia Commercial Zone
Sub-Zone	Area A3
Overlay	Pedestrian Frontage
Proposal	Land use consent to establish five relocatable cabins within 30 and 32 Selwyn Road to provide visitor accommodation and to provide a commercial parking area to accommodate seven car parking spaces and for the overnight parking of up five self-contained campervans within 32 and 34 Selwyn Road.
Activity Status	Restricted Discretionary Activity

Proposed Far North District Plan, Notified 2022

Zone	Mixed Use
Sub-Zone	Area B
Zone Overlay	Pedestrian Frontage
Environment	Coastal

Introduction

1. This application for resource consent is made pursuant to Section 88 of the Resource Management Act 1991 (the Act). The application is supported by an assessment of environmental effects as is required by section 88 of the Act, with the assessment relying on technical assessments and recommendations. The information contained in this application addresses the requirements set out in Schedule 4 of the Act.
2. The sites that are the subject to this application are located at 30, 32 and 34 Selwyn Road, Paihia. An existing retail shop is located at 30 Selwyn Road, 32 Selwyn Road is vacant, and 34 Selwyn Road contains an existing dwelling.
3. The applicant, Paihia Terraces Ltd ('PTL'), proposes to establish five relocatable cabins onto 30 and 32 Selwyn Road for visitor accommodation and provide a carparking area for up to seven cars (including one accessible parking space) associated with this activity. Within 32 Selwyn Road, it is proposed to provide a commercial carparking area for up to seven cars, and within 34 Selwyn Road, provide five overnight parking spaces for self-contained campervans.
4. The total combined area of the three sites is 2280m² and the sites are zoned Commercial under the Operative Far North District Plan and subject to a pedestrian frontage control.
5. This assessment concludes that any actual or potential adverse effects on the environment that may arise from the Far North District Council granting land use consent to the proposed land use activities will be less than minor and that no persons will be adversely affected.

Legal Description

6. The property comprises three sites that are legally described as:
 - Lot 1 DP 328135 (RT NA114614) and contains 760m² (34 Selwyn Road).
 - Lot 2 DP 328135 (RT NA114615) and contains 691m² (32 Selwyn Road).
 - Lot 3 DP 328135 (RT NA114616) and contains 829m² (30 Selwyn Road).
7. A copy of the above Record of Titles and relevant documents are provided in **Attachment 1**.

8. There is a Right of Way (Pedestrian Access), Rights to Convey Water, Telecommunications, Computer Media and Electricity and Rights to drain Sewerage and Water Easement over area 'A' over Lot 3 DP 328135 (30 Selwyn Road) in favour of Lot 4 DP 328135 (16 Bayview Road). It is noted that the easement is located around the edge of the existing retail shop.
9. There is a private land covenant in transfer that limits the height of buildings and vegetation to a maximum height of 12m on each of these sites and a private fencing covenant in transfer registered over each of these sites.
10. There are no other interests registered against the Record of Titles for these site that affect the proposed development.

Description of Site and Surrounding Environment

11. The site is located immediately adjacent to the commercial area within the Paihia township and one block inland from the Paihia wharf and Bay of Islands harbour. The site is located within an area that where visitor accommodation is the predominant activity, adjacent to the establish retail area and community facilities located within the town centre.
12. All of the sites contain flat to moderate sloping land adjacent to Selwyn Road, then the topography slopes upwards (moderate slopes) to the western boundary, beyond which, the land contains steeper vegetated slopes generally contained within 16 Bayview Road.
13. The existing 58m² building located at 30 Selwyn Road contains a retail shop and an established vehicle crossing, driveway and parking area is provided at the rear of the building. A verandah extends from the building over the footpath along Selwyn Road. The hillslope at the rear of the site is terraced and there is an elevated building platform available.
14. The dwelling at 32 Selwyn Road has been removed and this site is currently vacant. The site contains a vehicle crossing from Selwyn Road, but the crossing is currently in grass. The front portion of the site contains flatter land with a gentle gradient that extends upwards from the road frontage of the site before the land slopes more steeply upwards to the western boundary.

15. The site at 34 Selwyn Road contains an established two storey dwelling with on-site parking provided under the verandah. The dwelling is set back from the road, located on an elevated building platform located to the rear of the site. There is a formed vehicle crossing and concrete driveway that extends to the dwelling located adjacent to the northern site boundary.

16. The immediate surrounding area (opposite and directly adjacent) is characterised by large sites that provide visitor accommodation. The Paihia town centre is located south of the site. The western site boundary adjoins a 2605m² residential site that has frontage to Bayview Road (16 Bayview Road). The dwelling located on this site is located near the Bayview Road frontage whilst the rear of the site is in grass and mature trees and shrubs.

17. The aerial photograph below identifies the location of the sites and depicts the approximate extent of the property and boundaries.

30 – 24 Selwyn Road, Paihia



(Source: Far North District Council GIS Viewer).

18. Following a Pre-Application Meeting held on 3 October 2024 (CDM-2025-53), the meeting notes confirm that based on the FNDC Local Maps - HAIL Sites and the Northland Regional Council’s Selected Land Use Register, the sites are not identified as being contaminated in terms of the National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health (‘NESCS’).
19. The Far North District Council’s 3 Waters Map shows that public wastewater, water supply and stormwater lines are located within Selwyn Road.

30 – 34 Selwyn Road, Paihia



(Source: Far North District Council 3Water Maps: Red = wastewater, green = stormwater, blue = water supply).

20. There are no waterbodies on or near the site. The site is located at least 140m from Paihia Wharf and the Bay of Islands waterfront.
21. A review of the Far North District Council (‘FNDC’) and Northland Regional Council (‘NRC’) GIS Maps was undertaken to ascertain if there are any hazards that may affect the site, and none are recorded for these sites. Following a Pre-Application Meeting, the meeting notes recommend that full attenuation of stormwater be proposed due to the potential to affect flooding of downstream properties.

Proposal

22. The applicant is seeking resource consent to establish five relocatable cabins onto 30 and 32 Selwyn Road that will be used to provide visitor accommodation. Seven car parking spaces (including one accessible car parking space will be provided across both sites).
23. It is proposed that the flatter land adjacent to the road frontage of the sites at 32 and 34 Selwyn Road be used to provide commercial car parking (up to seven carparking spaces) and for the overnight parking of up to five self-contained campervans.
24. The commercial carparking area will be used to provide for all-day parking, and tickets will be designed as a single price for all-day parking.
25. The applicant seeks consent for to undertake these activities for a maximum duration of five years. Following the expiry of this consent, the applicant anticipates undertaking a comprehensive development across the site that will include both commercial and residential activities.
26. The proposed 30.6m² relocatable cabins will include two bedrooms, a bathroom and an open plan kitchen/dining and living area. They will be located within centre of 30 and 32 Selwyn Roads and staggered towards the rear of these sites. A concrete footpath will be provided from the carparking area to provide pedestrian access to the buildings. Steps and decking will facilitate access to the buildings as the cabins will be constructed using pile foundations.
27. The cabins within 30 Selwyn Road will be located at least 1.7m from the southern site boundary (outside of Right of Way Easement A), 5m from the rear boundary and 9.5m from the front boundary. The cabins within 32 Selwyn Road will be located at least 0.5m from the northern site boundary, 11m from the front boundary and over 8m from the western boundary.
28. A parking area for seven cars (including one accessible parking space) will be provided within the flatter land located near the frontage of the site. Landscaping will be provided along the frontage of these sites (refer **Attachment 3** - Landscape Plans).

29. The flatter portion of land within 32 and 34 Selwyn Roads will provide commercial carparking space for up to seven cars and five spaces to provide for overnight stays for self-contained campervans within the site.
30. A free-standing sign is proposed within the frontage of 32 Selwyn Road for wayfinding and information purposes. The sign will measure 3.6m wide by 1.2m high (with a total area of 4.32m²) and will be attached to 2.6m high posts.
31. There is an existing formed vehicle entrance to the site at 30 Selwyn Road and this will continue to be utilised to provide access to the parking area that will be provided to serve the visitor accommodation activities within 30 and 32 Selwyn Roads.
32. There is an informal vehicle crossing to 32 Selwyn Road (kerb formation visible, but the surface is in grass) that will be upgraded to provide vehicle access to the commercial parking spaces proposed across the front portions of 32 and 34 Selwyn Roads.
33. The existing dwelling will continue to utilise the existing vehicle crossing and driveway to 34 Selwyn Road and is therefore not affected by the proposal.
34. These temporary land use activities (maximum duration of five years) have been designed to minimise land disturbance and minimise effects on infrastructure. The parking areas will be gravelled to minimise land disturbance and minimise impervious surfaces within the site.
35. A Stormwater Management Report has been prepared (refer **Attachment 4**) that recommends that a new DN375 connection be made from the site to the existing public stormwater manhole (GIS ID:3505) on Selwyn Road. The pipe capacity analysis that was undertaken confirms that the existing DN600 stormwater pipe has adequate capacity to cater for the post-development runoff from the proposed development. As the post development peak flow through the pipe is less than the capacity of the pipe, the proposed development is expected to have a less than minor effect on the existing stormwater network.
36. The Stormwater Management Report also confirms that the proposed carpark on the ground floor will be mitigated using filtration based on the relevant standards.

37. Following the Pre-Application Guidance Meeting, the Resource Consents Engineer has advised that the site can connect to the public wastewater and water supply systems and has advised that booster pumps may be required to provide water to those cabins located towards the rear of the site.

Planning Assessment

Operative Far North District Plan 2009

38. An assessment of the proposal against the objectives, policies and relevant rules of the Operative Far North District Plan is provided in **Attachment 6**.
39. In summary:

Part 2 – Environment Provisions			
7.7 Commercial Zone			
Rule No.	Title	Standards	
7.7.5.1	Permitted Activities		
	<p><i>An activity is a permitted activity in the Commercial Zone if:</i></p> <p><i>(a) it complies with the standards for permitted activities set out in Rules 7.7.5.1.1 to 7.7.5.1.12 below; and</i></p> <p><i>(b) it complies with the relevant standards for permitted activities set out in Part 3 of the Plan - District Wide Provisions.</i></p>		
7.7.5.1.1	Building Height:	10m	Will comply
7.7.5.1.2	Sunlight		Not applicable
7.7.5.1.3	Visual Amenity and Environmental Protection		Not applicable
7.7.5.1.4	Setback from Boundaries 'Pedestrian Frontage':	All buildings to be built up to the road boundary and a veranda provided along the full frontage of the site that relates to neighbouring site to provide continuous pedestrian cover.	Does not comply
7.7.5.1.5	Noise Mitigation for Residential Activities:	Noise Mitigation for Residential Activities: Acoustic design report to be provided. Attenuation of noise between any boundary and living room is no less than 20dB and between any boundary and any room used for sleeping is no less than 30dB.	Will comply Consent condition
7.7.5.1.6	Traffic, Parking and Access	Refer Chapter 15 - Transportation	Will comply
7.7.5.1.7	Keeping of Animals		Not applicable

7.7.5.1.8	Noise	All activities within the zone shall be conducted so that noise measured at any point within any other site in the zone shall not exceed: 0700 to 2200 hours 65 dBA L ₁₀ 2200 to 0700 hours 55 dBA L ₁₀ and 80 dBA L _{max} .	Will comply.
7.7.5.1.10	Roof Pitch		Not applicable
7.7.5.1.11	Stormwater	The disposal of collected stormwater from the roof of all new buildings and new impervious surfaces provided that the activity is within an existing consented urban stormwater management plan or discharge consent.	Will comply.
7.7.5.1.12	Helicopter Landing Area		Not applicable
7.7.5.2	Controlled Activities		
	<p><i>An activity is a controlled activity in the Commercial Zone if:</i></p> <p><i>(a) it complies with all of the standards for permitted activities except for any one of the following Rules 7.7.5.1.2 Sunlight, 7.7.5.1.6 Transportation and 7.7.5.1.11 Stormwater above; and</i></p> <p><i>(b) it complies with Rules 7.7.5.2.1 Sunlight, 7.7.5.2.2 Transportation and Rule 7.7.5.2.3 Stormwater below; and</i></p> <p><i>(c) it complies with the relevant standards for permitted or controlled activities set out in Part 3 of the Plan - District Wide Provisions.</i></p> <p><i>The Council must approve an application for a land use consent for a controlled activity, but it may impose conditions on that consent.</i></p>		
7.7.5.2.1	Sunlight		Not applicable
7.7.5.2.2	Transportation	Refer Chapter 15 – Transportation for traffic, parking and access rules.	Will comply (refer assessment below for Chapter 15)
7.7.5.2.3	Stormwater	A stormwater management system is proposed to meet these standards.	Will comply
7.7.5.3	Restricted Discretionary Activities		
	<p><i>An activity is a restricted discretionary activity in the Commercial Zone if:</i></p> <p><i>(a) it does not comply with any one of the following Rules 7.7.5.1.1 Building Height; 7.7.5.1.3 Visual Amenity and Environmental Protection; 7.7.5.1.4 Setback from Boundaries; 7.7.5.1.5 Noise Mitigation for Residential Activities; 7.7.5.1.8 Noise; 7.7.5.2.1 Sunlight and/or 7.7.5.2.2 Transportation; as set out above; but</i></p> <p><i>(b) it complies with all of the other rules for permitted and controlled activities under Rules 7.7.5.1 and 7.7.5.2; and</i></p> <p><i>(c) it complies with Rules 7.7.5.3.1 Building Height; 7.7.5.3.2 Transportation; 7.7.5.3.3 Visual Amenity & Environmental Protection; 7.7.5.3.4 Setback from Boundaries; 7.7.5.3.5 Noise Mitigation for Residential Activities; 7.7.5.3.6 Noise and 7.7.5.3.7 Sunlight below; and</i></p> <p><i>(d) it complies with the relevant standards for permitted, controlled or restricted discretionary activities set out in Part 3 of the Plan - District Wide Provisions.</i></p> <p><i>The Council may approve or refuse an application for a restricted discretionary activity, and it may impose conditions on any consent.</i></p>		

	<i>In assessing an application for a restricted discretionary activity, the Council will restrict the exercise of its discretion to the specific matters listed for each rule below, or where there is no rule, to the specific matters listed below under the appropriate heading.</i>		
7.7.5.3.1	Building Height	Maximum height for Paihia – 10m	Complies
7.7.5.3.2	Transportation	Refer to Chapter 15 – Transportation for Traffic, Parking and Access rules.	Will comply (refer assessment below for Chapter 15)
7.7.5.3.3	Visual Amenity and Environmental Protection	In assessing an application resulting from a breach of Rule 7.7.5.1.3 Visual Amenity and Environmental Protection the matters to which the Council will restrict its discretion are: (a) the visual effect on property in the vicinity of the proposed commercial development; (b) the scale and nature of the landscape and the resources necessary for its maintenance.	Not applicable
7.7.5.3.4	Setback from Boundaries	In assessing an application resulting from a breach of Rule 7.7.5.1.4 Setback from Boundaries the matters to which the Council will restrict its discretion are: (a) where a pedestrian frontage is required, the extent to which the proposal is in keeping with the existing character and form of the street or road, in particular with the external scale, proportions and buildings on the site and on adjacent sites; (b) the extent to which the buildings and their use will impact on the public use and enjoyment of adjoining esplanade reserves and strips and adjacent coastal marine areas.	Will comply. Refer assessment below. Temporary activities proposed. Small scale buildings are proposed that will be used for similar activities that predominate within locality. The site is not located near an esplanade reserve or strip or the coastal marine area.
7.7.5.3.5	Noise Mitigation for Residential Activities		Will comply (consent condition)
7.7.5.3.6	Noise		Will comply
7.7.5.3.7	Sunlight		Not applicable
Part 3 District-Wide Provisions			
Chapter 12.2 Indigenous Flora and Fauna			
12.2.6.1.1	Indigenous Vegetation Clearance	<i>Notwithstanding any rule in the Plan to the contrary but subject to Rules 12.5.6.1.1, 12.5.6.1.3 and 12.5.6.2.2 in the Heritage section of this Plan, indigenous vegetation clearance is permitted throughout the District where the clearance is for any of the following purposes: (o) it involves the felling, trimming, damaging or removal of a tree or group of trees in an urban</i>	Will comply.

		<p>environment unless the tree or group of trees is—</p> <p>(A) specifically identified in the plan (refer to Chapter 12.5 and Appendix 1D); or</p> <p>(B) located within an area in the district that—</p> <p>(i) is a reserve (within the meaning of section 2(1) of the Reserves Act 1977); or</p> <p>(ii) is subject to a conservation management plan or conservation management strategy prepared in accordance with the Conservation Act 1987 or the Reserves Act 1977.</p>	
Chapter 12.4 Natural Hazards			
12.4.6.1.2	Fire Risk to Residential Dwellings	<p>(a) Residential units shall be located at least 20m away from the drip line of any trees in a naturally occurring or deliberately planted area of scrub or shrubland, woodlot or forest;</p> <p>(b) Any trees in a deliberately planted woodlot or forest shall be planted at least 20m away from any urban environment zone, Russell Township or Coastal Residential Zone boundary, excluding the replanting of plantation forests existing at July 2003.</p>	Will comply.
Chapter 15 Transportation			
Rule No.	Title	Standards	Permitted Activity
15.1.6A.1.	Maximum Daily One-Way Traffic Movements	<p><i>Commercial Zone:</i></p> <p>200 – Permitted Activity</p> <p>201-500 – Controlled Activity</p> <p>More than 500 – Restricted Discretionary Activity</p>	Less than 200 maximum daily one-way traffic movements.
15.1.6A.2.1	Traffic Intensity	<p><i>Residential:</i></p> <p>Standard Residential Unit – 10 per unit</p> <p>Home Unit/Town House – 7 per unit/house</p> <p><i>Casual Accommodation:</i></p> <p>Camping Grounds/Motor Camps: 3 per unit and/or two per camp site.</p>	<p>Will comply.</p> <p>3 x 5 units = 15</p> <p>2 x 5 campervans = 10</p> <p>1 x dwelling = 10</p> <p>Maximum: 35</p>
15.1.6B	Parking	<p><i>Residential:</i></p> <p>Standard Residential Unit – 2 per unit</p> <p>Home Unit/Town House – 2 per unit</p> <p><i>Casual Accommodation:</i></p> <p>Camping Grounds/Motor Camps: 1 per unit/camp site, plus 1 per two employees.</p>	<p>Will comply.</p> <p>Existing dwelling = 2 parking spaces</p> <p>5 units plus 5 campervan sites = 10 x 1 per site = 10</p> <p>Two employees = 1</p> <p>13 parking spaces.</p>

		<p><i>Accessible Car Parking Spaces:</i> Where 20 or less car parking spaces provided – then one accessible car parking space is to be provided. Where 21 – 50 car parking spaces are provided – then two accessible car parking spaces are to be provided.</p> <p><i>Loading Spaces:</i> Building GFA no greater than 200m² – no loading space is required to be provided. Building GFA between 200m² and 500m² – one loading space to be provided. Building GFA between 500m² and 5000m² – two loading spaces are to be provided. Building GFA exceeding 5000m² – three loading spaces are to be provided.</p> <p>All parking, loading and access drives and manoeuvring areas are to be formed and provided with an all-weather surface, drained and marked out.</p>	<p>Fourteen parking spaces proposed including one accessible parking space.</p> <p>No loading space is required as each site will contain a building gross floor area of less than 200m².</p> <p>Gravelled surface proposed across the parking, loading and access drives within the site that will provide for drainage and will be marked out.</p>
15.1.6C	Access	<p>A private access may serve a maximum of 8 household equivalents.</p> <p>One-way private accessway from the road – not less than 3m or more than 4m in width and have a minimum overhead clearance of 4.2m.</p> <p>Two-way private accessway from the road – shall not be less than 6m or more than 7m in width and have a minimum overhead clearance of 4.2m.</p> <p>Maximum width of vehicle access over footpaths: 6m. Maximum of two crossings per site.</p> <p>Accessway to be no steeper than 1:20 adjacent to the road boundary for a length of at least 6m.</p> <p>Stormwater management required.</p> <p>Accessways to be constructed in accordance with the Council's <i>Engineering Standards and Guidelines</i> (June 2004 – Revised 2009).</p>	<p>Each site has an existing vehicle crossing.</p> <p>Maximum width of the vehicle crossings is 4.5m.</p> <p>Complies.</p> <p>Stormwater management will be provided.</p> <p>Will comply.</p>
Chapter 16 Signs and Lighting			
16.6.1.2	General Requirements for all Signs	<p><i>Commercial Zone</i> Maximum height: (including support structure): the maximum height of any signs attached to, or</p>	Will comply.

		<p>displayed on a building or veranda may exceed 4m but shall not exceed the height of the building or otherwise provided for in Rules 16.6.1.4 or 16.6.1.5.</p> <p>Maximum Number of Freestanding Signs: no limits or otherwise restricted or provided by in Rules 16.6.1.4 or 16.6.1.5.</p>	
16.6.1.3	Maximum Sign Area per Site	<p><i>Commercial Zone:</i></p> <p>For each road frontage:</p> <p>Less than 24m: 6m²</p> <p>Greater than 24m: 0.25m² for every 1m of road frontage, up to 12m².</p>	<p>Will comply – the road frontage of 32 Selwyn Road is 13.72m and the area of the proposed sign is 4.32m².</p>

40. The assessment identifies that the proposed development does not comply with the Rule 7.7.5.1.4 Setback from Boundaries ‘Pedestrian Frontage’ and resource consent is therefore triggered for a **Restricted Discretionary Activity**.

Assessment of Effects

Character and Amenity

41. The proposed activity includes visitor accommodation and the provision of commercial carparking spaces/overnight parking for self-contained campervans within these sites. A maximum duration of consent for up to five years is sought, after which, the applicant intends to undertake a comprehensive development of the property (across the three sites).
42. The proposed activities have been designed and located to utilise existing infrastructure available to the property including existing vehicle crossings and to minimise land disturbance.
43. Visitor accommodation is the predominant activity within the immediate locality. As such the proposal to provide for visitor accommodation and for visitor parking (on a commercial basis) is consistent with the established character and amenity of the area.
44. On site parking will be provided and a gravelled surface proposed to minimise stormwater runoff and minimise land disturbance.

45. A landscape plan has been prepared for planting along the frontage of the site and around the proposed cabins that will contribute positively to the streetscape and amenity of the commercial zone/town centre.
46. The scale and nature of the built development proposed is considered to be compatible with the character of the area and would not contribute to adverse reverse sensitivity effects. It is recommended that resource consent include a consent condition relating to the control of noise from land use activities undertaken within the site.
47. The location of the proposed buildings and parking areas within the site have been designed to avoid adverse effects on access to sunlight, and to mitigate potential adverse effects associated with noise and stormwater runoff on adjoining properties.
48. The development complies with the maximum height standard and landscape planting is proposed along the street frontage to mitigate the visual and amenity effects of the location of the buildings and parking areas upon the streetscape.
49. The proposal has been designed to contribute towards the range of activities provided within the Commercial zone in a manner that will maintain the streetscape and amenity anticipated whilst accommodating the needs and well-being of visitors to the Paihia town centre.
50. Overall, it is considered that the amenity and character effects of the development will be less than minor.

Landscape and Visual Effects

51. It is considered that visual effects will be less than minor given the:
 - small scale and temporary nature of the activities proposed;
 - light 'footprint' associated with the activities (with the relocatable cabins on pile foundations and gravel parking areas provided to minimise impervious surfaces/land disturbance); and
 - landscape planting proposed to 'soften' the development and contribute to the amenity of the streetscape.

52. It is considered that the proposed development will not appear visually dominant given that they will be undertaken on the flatter areas of the site, include small scale and single storey buildings.
53. The buildings and parking areas have been designed to minimise land disturbance and avoid changes to the landform. The proposed activities will be undertaken within the flatter areas of the site, the cabins will be located on pile foundations and the parking areas formed using gravel to minimise land disturbance and minimise impervious surfaces.
54. As outlined above, the proposed landscape planting along the frontage of the site and around the cabins will mitigate the visual effect of the proposed development and contribute positively to the streetscape.

Traffic Effects

55. It is considered that the traffic generation associated with the proposed activities will not be significant in the context of the existing traffic environment. The proposed activities are small in scale and land use consent is sought is for a maximum duration of 5 years.
56. The applicant has prepared a traffic generation assessment for the proposed activities (refer **Attachment 5**) where the total daily trips per day is 64. This is consistent with traffic generated by similar small-scale tourist accommodation activities and complies with the thresholds for traffic intensity in the District Plan.
57. It is considered that the traffic generated by the proposal can be accommodated on the road network without compromising its function, capacity or safety. It is reasonable to anticipated that these activities can be anticipated within the Commercial zone, and the traffic impact assessment demonstrates that the low volume of traffic generated by the activities will not adversely affect the local roading network or require traffic mitigation measures.
58. Footpaths to facilitate the movement of pedestrians within the site has been provided for. There is a formed footpath located within the adjacent road reserve.

59. The proposal includes upgrading of the existing vehicle crossing to 32 Selwyn Road to facilitate access to the proposed parking area for cars and campervans within the site. A vehicle crossing will be constructed in accordance with the Far North District Council Engineering Standards as set out in Standard 15.1.6C.1.6 *Vehicle Crossing Standards in Urban Zones*.
60. As the vehicle crossing that is to be upgraded is associated with residential related activities, the Engineering Standard for Residential Vehicle Crossings has been used. This will minimise potential effects upon the parking spaces provided within Selwyn Road adjacent to the site and appropriate to serve the proposed land use activities. It is noted that Rule 15.1.6C.1.6 Vehicle Crossing Standards in Urban Zones in the District Plan references the Council's "*Engineering Standards and Guidelines*" (June 2004 – Revised 2009).
61. The Vehicle Crossing in the 2009 revised version of the Engineering Standards includes Sheet FNDC / S / 2 for *Residential and Commercial Vehicle Crossings* and that the Far North District Council Engineering Standards have been further updated, Version 0.6 2023.
62. Given the small scale and nature of the proposed activities, the limited duration of consent sought and that the new crossing will need to be removed when the applicant undertakes the comprehensive development proposed across the site, that the proposed Vehicle Crossing - Residential is appropriate and consistent with the outcomes anticipated by the District Plan and the Engineering Standards.
63. The provision of seven car parking spaces for the proposed visitor accommodation activities and five parking spaces for overnight parking of self-contained campervans complies with the parking standard for camping grounds/motor camps in the District Plan. All access drives and manoeuvring aisles will be formed (gravel) and parking spaces will be marked out.
64. The applicant provided a traffic impact assessment for the proposed commercial car parking activity, which calculates that 14 vehicle trips per day will be generated. The carparking spaces, access and manoeuvring aisle have been designed to comply with the standards for traffic, parking and access in Chapter 15 of the District Plan.
65. Overall, it is considered that the small scale and nature of the proposed activities, that traffic effects are expected to be less than minor.

Effects on Infrastructure

66. Public services are available to the site. Following a Pre-Application Guidance Meeting, correspondence has been received that confirms that connections to the public stormwater and wastewater reticulation systems can be achieved and that water booster pumps may be required for the provision of public water supply to those cabins located on the lower hillslopes within the site.
67. A Stormwater Management Assessment Report has been prepared that recommends that the public stormwater connection be replaced by a new DN375 pipe to serve the cabins and the carparks. The pipe capacity assessment confirms that there is adequate capacity within the network to accommodate the proposed development.
68. It is considered that suitable and sufficient infrastructure can be put in place to service the proposed development, and any infrastructure effects will be less than minor.

Construction Effects

69. The proposed works present a low risk in terms of erosion and sediment effects due to the flat gradient of the site and the low volume of earthworks. The site is well suited for the form of development proposed and adverse earthworks and site preparation effects can be avoided, remedied and mitigated to be less than minor.
70. Noise associated with construction activities are inevitable. The proposal includes establishing five relocatable cabins within the site, rather than constructing these buildings on site, which will mitigate potential noise effects. Furthermore, noise from the construction activities will be temporary in nature, and the contractors will operate under standard best practice construction methodologies to ensure that unreasonable noise is avoided.
71. Overall, it is expected that site works will be managed to ensure any adverse construction effects will be less than minor.

Consultation

72. It is recognised good practice to consult. Furthermore, the Fourth Schedule (Clause 6(1f)) of the Resource Management Act states that an Assessment of Environmental Effects should include a statement that identifies those persons affected by the activity, details of any consultation undertaken, and any response to the views of those consulted.
73. It is considered that no persons are adversely affected by the development given the small scale and limited duration of consent sought for the proposed land use development. The potential amenity effects of the proposed development are considered less than minor. The development will not appear visually obtrusive, given the small scale and nature of the activities and that minimal land disturbance is required. The buildings and land use activities have been located within the flatter areas within the site and landscape planting will be provided. Appropriate infrastructure is available to service the proposed development and additional stormwater management controls are proposed to reduce stormwater runoff from impervious surfaces to 80% of pre-development levels.
74. A Pre-Application Guidance Meeting was held with Yuna Zhou (Intermediate Resource Planner) and Ishan Koshatwar (Resource Consents Engineer) on 3 October 2024. The proposal has been amended in accordance with the guidance provided by Council following the meeting.

Resource Management Act 1991

Section 95

75. Relevant notification procedures for this resource consent application are specified in Sections 95A-G of the Act. The Resource Legislation Amendments Act 2017 has changed the statutory tests to determine notification. A four-step process is to be followed to firstly determine public notification and then limited notification. These changes came into force on 18 October 2017. The Resource Management Amendment Act 2020 (2020 No 30) repealed or amended some of the steps set out in sections 95A and 95B, effective on 30 September 2020, as noted below.

76. An assessment of these steps to determine notification is provided below:

Resource Management Act	Assessment
95A Public Notification of consent applications	
Step 1: Mandatory public notification in certain circumstances	
<i>The applicant has requested that the application be publicly notified.</i>	The applicant does not request public notification.
<i>Public notification is required under section 95C Public Notification of consent application after request for further information or report.</i>	To date there has been no request for further information from the Council.
<i>The application is made jointly with an application to exchange recreation reserve land under section 15AA of the Reserves Act 1977.</i>	The application does not include the exchange of recreation reserve land.
Step 2: If not required by Step 1, Public Notification Precluded in Certain Circumstances	
<i>The application is for a resource consent for one or more activities, and each activity is subject to a rule or national environmental standard that precludes public notification.</i>	None of the activities are subject to a rule that precludes public notification.
<i>The application is for a resource consent for one or more of the following, but no other, activities:</i> (i) a controlled activity: (ii) [Repealed] (iii) a restricted discretionary, discretionary, or non-complying activity, but only if the activity is a boundary activity: (iv) [Repealed]	Overall, the activity has a Restricted Discretionary Activity status but is not a boundary activity.
Step 3: if not precluded by step 2, public notification required in certain circumstances	
<i>The application is for a resource consent for one or more activities, and any of those activities is subject to a rule or national environmental standard that requires public notification.</i>	Not Applicable
<i>The consent authority decides, in accordance with section 95D¹, that the activity will have or is likely to have adverse effects on the environment that are more than minor.</i>	Refer to the Assessment of Environmental Effects provided above. It is considered that the proposal will not have, or is not likely to have, adverse effects on the environment that are more than minor.
Step 4: Public Notification in Special Circumstances	
<i>Determine whether special circumstances exist in relation to the application that warrant the application being publicly notified.</i>	No special circumstances exist in relation to the application to warrant public notification.

¹ Section 95D 'Consent Authority decides if Adverse Effects Likely to be More than Minor' of the Resource Management Act 1991.

<p><u>It is concluded that Public Notification is not required.</u></p>	
<p>Next step is to determine whether limited notification is required.</p>	
<p>95B Limited Notification of Consent Applications</p>	
<p>Step 1: Certain affected groups and affected persons must be notified</p>	
<p><i>Determine whether there are any—</i> <i>(a) affected protected customary rights groups; or</i> <i>(b) affected customary marine title groups (in the case of an application for a resource consent for an accommodated activity).</i></p>	<p>There are no known affected protected customary rights groups or customary marine title groups relevant to this application or site.</p>
<p><i>Determine—</i> <i>(a) whether the proposed activity is on or adjacent to, or may affect, land that is the subject of a statutory acknowledgement made in accordance with an Act specified in Schedule 11; and</i> <i>(b) whether the person to whom the statutory acknowledgement is made is an affected person under section 95E.</i></p>	<p>The land is not on or adjacent to or may affect land subject to a statutory acknowledgement made in accordance with an Act specified in Schedule 11.</p>
<p>Step 2: if not required by step 1, limited notification precluded in certain circumstances</p>	
<p><i>The application is for a resource consent for one or more activities, and each activity is subject to a rule or national environmental standard that precludes limited notification:</i></p>	<p>The activity is not subject to a rule that precludes limited notification.</p>
<p><i>The application is for a controlled activity (but no other activities) that requires a resource consent under a district plan (other than a subdivision of land).</i></p>	<p>The overall activity status is Restricted Discretionary.</p>
<p>Step 3: if not precluded by step 2, certain other affected persons must be notified.</p>	
<p><i>In the case of a boundary activity, determine in accordance with section 95E whether an owner of an allotment with an infringed boundary is an affected person.</i></p>	<p>Not applicable. The activity is not a boundary activity.</p>
<p><i>In the case of any other activity, determine whether a person is an affected person in accordance with section 95E.</i></p> <p><i>A person is an affected person if the consent authority decides that the activity's adverse effects on the person are minor or more than minor (but are not less than minor).</i></p>	<p>No persons are considered adversely affected by the activity as any adverse effects will be less than minor.</p> <p>The proposed land use activities represent an efficient use of commercial zoned land for a maximum duration of five years, utilising existing infrastructure whilst minimising land disturbance.</p> <p>The proposed development is of a small scale and has been designed to maintain amenity values and the landscape planting</p>

	<p>proposed will contribute positively towards the streetscape.</p> <p>A Stormwater Management Report recommends that a replacement stormwater pipe be installed to serve the cabins and carparks. The pipe capacity assessment undertaken confirms that there is adequate capacity within the stormwater network to accommodate the proposed development.</p> <p>No persons are considered to be adversely affected by the proposal.</p>
<p>Step 4: Further notification in special circumstances</p>	
<p><i>Determine whether special circumstances exist in relation to the application that warrant notification of the application to any other persons not already determined to be eligible for limited notification under this section (excluding persons assessed under section 95E as not being affected persons),</i></p>	<p>No special circumstances exist in relation to the application to warrant public notification.</p>
<p><i><u>It is concluded that the application can proceed on a Non-Notified Basis.</u></i></p>	

Section 104

77. When considering an application for resource consent, Council pursuant to Section 104 and subject to Part 2 of the Act must have regard to:

- *s104(1)(a) Any actual or potential effect on the environment of allowing the activity; and*
- *s104(1)(ab) Any measure proposed or agreed to by the applicant for the purpose of ensuring positive effects on the environment to offset or compensate for any adverse effects on the environment that will or may result from allowing the activity; and*
- *s104(1)(b) Any relevant provisions of a national policy statement, New Zealand coastal policy statement, regional policy statement, plan or propose plan; and*

- *s104(1)(c) Any other matter Council considers relevant and reasonably necessary to determine the application.*

Section 104(1)(a)

78. Section 104 requires an overall balancing exercise of a range of considerations, one of which is the actual and potential effects, on the environment of allowing the activity (s104(1)(a)).
79. It is our opinion that the proposed development represents an efficient use of the land resource, and the proposed activities will not generate adverse amenity effects.
80. It is submitted that any actual or potential effects on the environment from granting consent to the application will be less than minor and that adverse effects can be avoided or mitigated.
81. The proposed relocatable cabins can be adequately serviced by the public reticulation and additional on-site stormwater controls are proposed to control stormwater runoff from impervious areas to 80% of pre-development levels and avoid downstream flooding effects.

Positive Effects

82. The development will efficiently utilise Commercial zoned land located within the Paihia town centre to accommodate land use activities that are of a small scale and nature that will contribute to the range of activities available to meet the needs and well-being of the community in a manner that will protect the amenity of the area.

Section 104(1)(b)

83. In terms of those matters set out in section 104(1)(b) we note that:
- The following National Policy Statements are not relevant:
 - National Policy Statement for Electricity Transmission,
 - National Policy Statement for Renewable Electricity Generation,
 - National Policy Statement for Highly Productive Land,

- National Policy Statement for Indigenous Biodiversity,
 - National Policy Statement for Greenhouse Gas Emissions from Industrial Process Heat.
- The proposal is not contrary to the direction contained in the National Policy Statement on Urban Development, as the site is located within an urban environment and the activities proposed are consistent with those anticipated within the Commercial zone and the proposal includes mitigation measures to maintain amenity. The small scale and nature of the proposed development will not preclude the site from being further developed in the near future.
 - The National Policy Statement for Freshwater Management is relevant to the extent that the development through adoption of a suitable stormwater management system will not have any adverse effects upon water quality.
 - The New Zealand Coastal Policy Statement is not relevant in that the site is located over 140m from the waterfront.
84. The National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health is not relevant in that the site is not recorded in either of the land use registers for HAIL sites held by the Far North District Council and Northland Regional Council. The design and location of the proposed development within the site minimises the land disturbance required.
85. The proposed development will not be contrary to the Regional Policy Statement ('RPS') regarding subdivision, use and development or any Proposed Plan Change associated with this or other relevant planning documents.
86. Objective 3.11 of the RPS for Northland seeks to achieve a regional form that have vibrant, safe and cohesive town centres with a range of residential or business opportunities. The urban areas are expected to be developed in a manner that promotes sustainable management of natural and physical resources, while preserving the distinctive character and amenity of each area. It is considered that the proposed development within the Paihia town centre contributes towards the range of commercial activities provided within the town centre in a manner that will maintain the amenity and character of the area.

87. The proposal is consistent with the relevant objectives and policies of the Operative Far North District Plan. In summary:

- The small scale and nature of the proposed development is consistent with the range of activities anticipated within the Commercial zone.
- The proposed development will efficiently utilise Commercial zoned land in a manner that minimises visual and environmental effects whilst maintaining amenity.
- The development can be adequately serviced and can connect to the existing water supply, wastewater and stormwater networks.
- The proposed development will visually integrate with and will maintain amenity values of the surrounding the area. Landscape planting is proposed to maintain amenity values and positively contribute to the streetscape.
- Stormwater management devices are proposed that will control stormwater runoff from impervious surfaces to achieve 80% of pre-development levels in compliance with the District Plan standards.
- The small scale and nature of the land use activities proposed ensures that traffic generated by the proposed development complies with the maximum traffic intensity standards in the District Plan.

Section 104(1)(c) Other Matters

Proposed Far North District Plan – Notified 2022

88. The meeting minutes from the Pre-Application Guidance Meeting, contained the following advice:

“The sites fall within the Mixed Use Zone under the Proposed District Plan, with no provisions having legal effect currently. But its objectives and policies need to be assessed in the AEE report.”

89. An assessment of the proposal in terms of the objectives and policies of the Proposed Far North District Plan is provided in **Attachment 8**.

90. It is noted that visitor accommodation is the predominant land use activity in the immediate vicinity of the site. The overview for the Mixed Use zone in the Proposed Far North District Plan indicates that the zone seeks to provide a framework in which commercial and residential activities can co-exist and enable a range of compatible activities.
91. The site is currently underutilised and is located within close proximity to the Paihia town centre. The proposal includes visitor accommodation and commercial parking to cater for tourists visiting the area. The consent seeks a maximum duration of five years for these activities, where upon the applicant intends to undertake a comprehensive development across these sites in a manner that will be consistent with the direction that has been outlined for the zone in the Proposed Far North District Plan.
92. The proposed activities will contribute towards the vibrancy, safety and amenity of the Mixed Use zone, noting that it is currently underutilised. Landscape planting is proposed along the frontage of the site and around the cabins that will contribute positively to the amenity and streetscape.
93. The objectives for the Mixed Use zone includes providing for residential development where it complements and is compatible with commercial, community and civic activities. Given the small scale and nature of the proposed activities and that they will be located within an area predominated by visitor accommodation activities, the proposal is not considered to be contrary to the direction for the Mixed Use zone under the Proposed District Plan.
94. It is noted that the direction contained in the Proposed District Plan is for residential activities to be accommodated above commercial activities to ensure active street frontages. The proposed residential activities are proposed towards the mid to rear of the site, with landscape planting proposed along the road frontage to maintain amenity and the streetscape.
95. The proposed development is able to utilise existing infrastructure available, provide for pedestrian amenity and the movement of people within, and to and from the site. Traffic generated is within the standard relating to traffic intensity thresholds and can be accommodated within the local roading network. Accommodation and parking areas will be provided within the site to cater for the needs of visitors to the town centre.
96. Appropriate mitigation measures are proposed to maintain amenity values and to avoid adverse privacy, shadowing or visual dominance effects.

Part 2 of the RMA

97. When a consent authority is considering a resource consent application in accordance with section 104 of the Resource Management Act 1991 ('the Act'), consideration must be given to Part 2 of the Act. Part 2 sets out the Purpose and Principles of the Act and these are contained in sections 5, 6, 7 and 8 of the Act.

Section 5 – Purpose

98. Section 5 outlines the purpose of the Act as being the promotion of sustainable management of natural and physical resources, and then proceeds to outline what is meant by sustainable management. This purpose is promoted through the objectives and policies of district and regional planning documents.

99. Sustainable management is not solely about avoiding adverse effects upon the environment; instead, there is an explicit acknowledgement in this section of the Act that adverse effects arise that can be remedied where they already exist and mitigated where they are anticipated. In addition to this, Section 5 anticipates that use, development and protection of natural resources will be managed in a manner that enables people to provide for the social, economic and cultural well-being.

100. Taking into account the definition of sustainable management contained in section 5(2), it is considered that the proposal does achieve the purpose of the Act. "Sustainable management" means managing the use, development and protection of natural and physical resources within certain parameters.

101. It is considered that the proposed activities represents an efficient use of physical land resource, as it facilitates the use of commercial zoned land for activities that are of a small scale and nature that for a limited duration to meet the reasonably foreseeable needs whilst allowing for the site to be redeveloped in a manner that has been designed to meet the needs of future generations.

102. The proposed 'interim use' (maximum duration of five years) of the site enables the applicant to provide for the social, economic and cultural well-being of people and communities and for their health and safety whilst avoiding, remedying or mitigating adverse effects on the environment.

103. The proposed scale and nature of the development within the site will utilise existing infrastructure, adopts a light building footprint to minimise land disturbance and impervious surfaces within the site, consistent with the purpose of the Act including the matters listed in section 5(2)(a), (b) and (c).

Section 6 – Matters of National Importance

104. Section 6 of the Act sets out a number of matters of national importance. In this circumstance:

- a) The site is located approximately 140m from the foreshore, and under the Proposed District Plan, it is within a Coastal Environment Overlay Area. The site located within an established urban development with visitor accommodation activities the predominant land use. The low-rise, small scale and nature of the proposed development ensures that the activities will not affect the natural character of the coastal environment, s6(a).
- b) There are no outstanding natural features or landscapes to be protected, s6(b).
- c) There are no significant indigenous vegetation or significant habitats to be protected, s6(c).
- d) The activity is not known to compromise the relationship of Māori and their culture and traditions with their ancestral lands, water, sites, waahi tapu, and other taonga, s6(e) being separated from identified sites of value.
- e) The proposal has taken into consideration the management of significant risks from natural hazards.
- f) The activity does not conflict with any of the matters of national importance to be recognised and provided for in achieving the sustainable management purpose of the Act.

Section 7 – Other Matters

105. Section 7 directs that in achieving the purpose of the Act, that the consent authority must have particular regard to certain matters which include, of relevance here, the efficient use and development of natural and physical resources, the maintenance and enhancement of amenity values; and the maintenance and enhancement of the quality of the environment.
106. The proposed activities represents an efficient use and development of natural and physical resources in a manner that does not compromise the ability of the site to accommodate a comprehensive mixed use development in the future.
107. The proposed activities will utilise existing infrastructure and the proposed landscape planting along the road frontage and around the cabins will contribute positively towards the maintenance and enhancement of amenity values, and the maintenance and enhancement of the quality of the environment.

Section 8 – Principles of the Treaty of Waitangi

108. We do not anticipate that the application would challenge the principles of the Treaty of Waitangi.

Conclusion

109. The proposed activities include a 'light building footprint' with five 30.6m² relocatable cabins proposed and gravel parking areas provided, minimising land disturbance, avoiding changes to the landform and minimises landscape and visual effects. Landscape planting is proposed along the road frontage and around the cabins to maintain amenity and contribute to the streetscape.
110. The proposed development is of a small scale and nature, with consent sought for a maximum duration of five years, after which the applicant intends to undertake a comprehensive mixed use development across the site. The assessment of environmental effects demonstrates that any adverse effects will be less than minor, with mitigation measures including landscape planting along the road frontage and around the cabins to maintain amenity and contribute positively to the streetscape.

111. An assessment of the proposal against the relevant objectives and policies and provisions of the Operative Far North District Plan has been undertaken. It is concluded that the proposal will be consistent with the outcomes anticipated for development within the Commercial zone, particularly given the small scale and nature of the proposed development and the mitigation measures that have been proposed.
112. In accordance with the recommendations contained in the meeting notes from the Pre-Application Guidance Meeting (refer **Attachment 6**), it is anticipated that a consent condition for the control of noise associated with the proposed activities will be imposed.
113. It is anticipated that a condition of consent will require that the existing public stormwater connection be replaced by a new DN375 pipe to serve the cabins and carparks, and that stormwater runoff from the car parking areas be mitigated using filtration based on the relevant standards as recommended in the Stormwater Management Report (refer **Attachment 4**).
114. An assessment of the proposal against the objectives and policies of the Proposed Far North District Plan 2022 has been provided, and it is considered that the proposed activities will not be contrary to the direction for the Mixed Use zone in this planning document. The proposal seeks consent for a maximum duration of five years, after which the applicant intends to undertake a comprehensive mixed use development across the site in manner that is consistent with the outcomes anticipated within the Mixed Use zone under the Proposed Far North District Plan.
115. As outlined above, the surrounding area is predominantly used for visitor accommodation and associated on-site parking, and therefore it is considered that the proposed residential and parking activities is consistent with the character of the area and the mitigation measures proposed will maintain the amenity of the area as anticipated by the District Plan.
116. It is recommended that consent be granted to the proposal with the imposition of appropriate conditions of consent.

Hodgson Planning Consultants



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R. W. Muir
Registrar-General
of Land

Identifier **114615**
Land Registration District **North Auckland**
Date Issued 04 November 2003

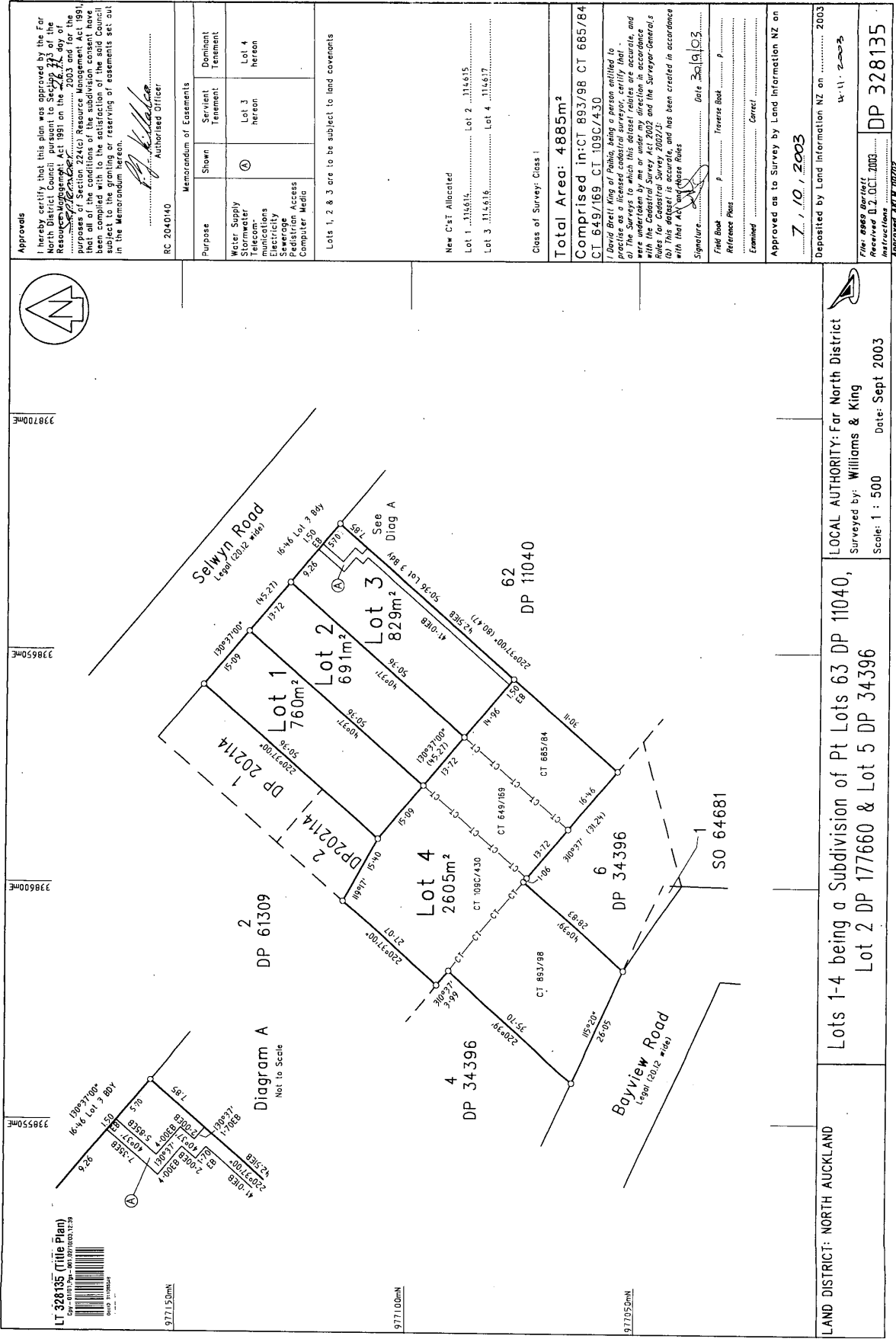
Prior References
NA649/169

Estate Fee Simple
Area 691 square metres more or less
Legal Description Lot 2 Deposited Plan 328135

Registered Owners
Paihia Terraces Limited

Interests

Land Covenant in Transfer 5830709.1 - 9.12.2003 at 9:00 am
Fencing Covenant in Transfer 5830709.1 - 9.12.2003 at 9:00 am
5959168.1 Variation of the land covenant specified in Transfer 5830709.1 - 7.4.2004 at 9:00 am
13032559.3 Mortgage to HP Capital Limited - 30.8.2024 at 5:19 pm



Approvals
 I hereby certify that this plan was approved by the Far North District Council pursuant to Section 223 of the Resource Management Act 1991 on the 26th day of September 2003 and for the purposes of the Resource Management Act 1991 that all of the conditions of the subdivision consent have been complied with to the satisfaction of the said Council subject to the granting or reserving of easements set out in the Memorandum hereon.

[Signature]
 Authorised Officer

RC 204/0140

Memorandum of Easements		
Purpose	Shown	Servient Tenement
Water Supply	Ⓐ	Lot 3 hereon
Stormwater		Lot 4 hereon
Telecom- munciations		
Electricity		
Pedestrian Access		
Computer Media		

Lots 1, 2 & 3 are to be subject to land coverants

New C&T Allocated
 Lot 1 114615
 Lot 2 114615
 Lot 3 114615

Class of Survey: Class 1

Total Area: 4885m²
Comprised in: CT 893/98 CT 685/84 CT 649/169 CT 109C/430

[Signature]
 Date 26/9/03

Field Book
 Reference Plans
 Examined
 Approved as to Survey by Land Information NZ on 7.10.2003

Deposited by Land Information NZ on 14.11.2003

File: 8889 Bartlett
 Received: 12 OCT 2003
 Instructions: DP 328135

APPROVED PLAN 00002

LAND DISTRICT: NORTH AUCKLAND

Lots 1-4 being a Subdivision of Pt Lots 63 DP 11040, Lot 2 DP 177660 & Lot 5 DP 34396

LOCAL AUTHORITY: Far North District
 Surveyed by: Williams & King
 Scale: 1 : 500 Date: Sept 2003



**RECORD OF TITLE
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R. W. Muir
Registrar-General
of Land

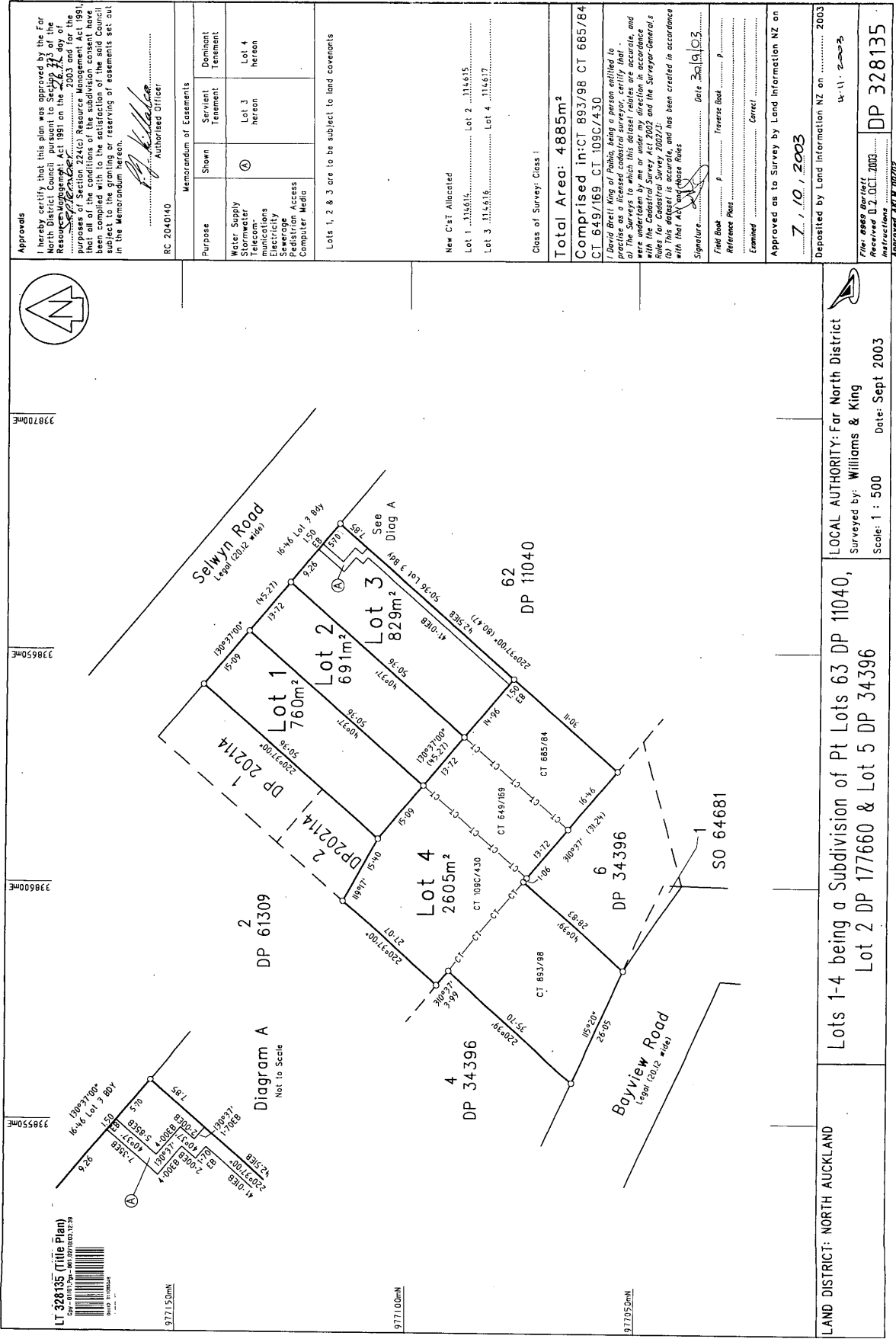
Identifier **114614**
Land Registration District **North Auckland**
Date Issued 04 November 2003

Prior References
NA109C/430

Estate Fee Simple
Area 760 square metres more or less
Legal Description Lot 1 Deposited Plan 328135
Registered Owners
Paihia Terraces Limited

Interests

Land Covenant in Transfer 5830709.1 - 9.12.2003 at 9:00 am
Fencing Covenant in Transfer 5830709.1 - 9.12.2003 at 9:00 am
5959168.1 Variation of the land covenant specified in Transfer 5830709.1 - 7.4.2004 at 9:00 am
13032559.3 Mortgage to HP Capital Limited - 30.8.2024 at 5:19 pm



Approvals
 I hereby certify that this plan was approved by the Far North District Council pursuant to Section 223 of the Resource Management Act 1991 on the 26th day of September 2003 and for the purposes of the Resource Management Act 1991 that all of the conditions of the subdivision consent have been complied with to the satisfaction of the said Council subject to the granting or reserving of easements set out in the Memorandum hereon.

[Signature]
 Authorised Officer

RC 204-D140

Memorandum of Easements		Dominant Tenement
Purpose	Shown	Servient Tenement
Water Supply Stormwater Telecom- munications Electricity Sewerage Pedestrian Access Computer Media	(A)	Lot 3 hereon Lot 4 hereon

Lots 1, 2 & 3 are to be subject to land coverants

New C&T Allocated
 Lot 1 1116616
 Lot 2 1116615
 Lot 3 1116616
 Lot 4 1116617

Class of Survey: Class 1

Total Area: 4885m²

Comprised in: CT 893/98 CT 685/84
 CT 649/169 CT 109C/430

I, David Beel, King of Pahia, being a person entitled to practice as a licensed cadastral surveyor, certify that the surveys to which this dataset relates are accurate, and were undertaken by me or under my direction in accordance with the Cadastral Survey Act 2002 and the Survey-General's Rules 2002. I declare that this dataset is accurate, and has been created in accordance with that Act and these Rules.

Signature: *[Signature]* Date: 26/09/03

Field Book P
 Traverse Book P
 Reference Plans P
 Examined Correct

Approved as to Survey by Land Information NZ on 7.10.2003

Deposited by Land Information NZ on 14.11.2003

File: 8889 Bartlett
 Received: 12 OCT 2003
 Instructions: DP 328135

APPROVED PLAN 2003

LAND DISTRICT: NORTH AUCKLAND

LOCAL AUTHORITY: Far North District
 Surveyed by: Williams & King
 Scale: 1 : 500 Date: Sept 2003

Lots 1-4 being a Subdivision of Pt Lots 63 DP 11040,
 Lot 2 DP 177660 & Lot 5 DP 34396



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R. W. Muir
Registrar-General
of Land

Identifier **114616**
Land Registration District **North Auckland**
Date Issued 04 November 2003

Prior References
NA685/84

Estate Fee Simple
Area 829 square metres more or less
Legal Description Lot 3 Deposited Plan 328135
Registered Owners
Paihia Terraces Limited

Interests

Fencing Agreement in Transfer 241927

Subject to a right of way (pedestrian access), rights to convey water, telecommunications, computer media and electricity, and rights to drain sewerage and water over part marked A on DP 328135 created by Easement Instrument 5785479.5 - 4.11.2003 at 9:00 am

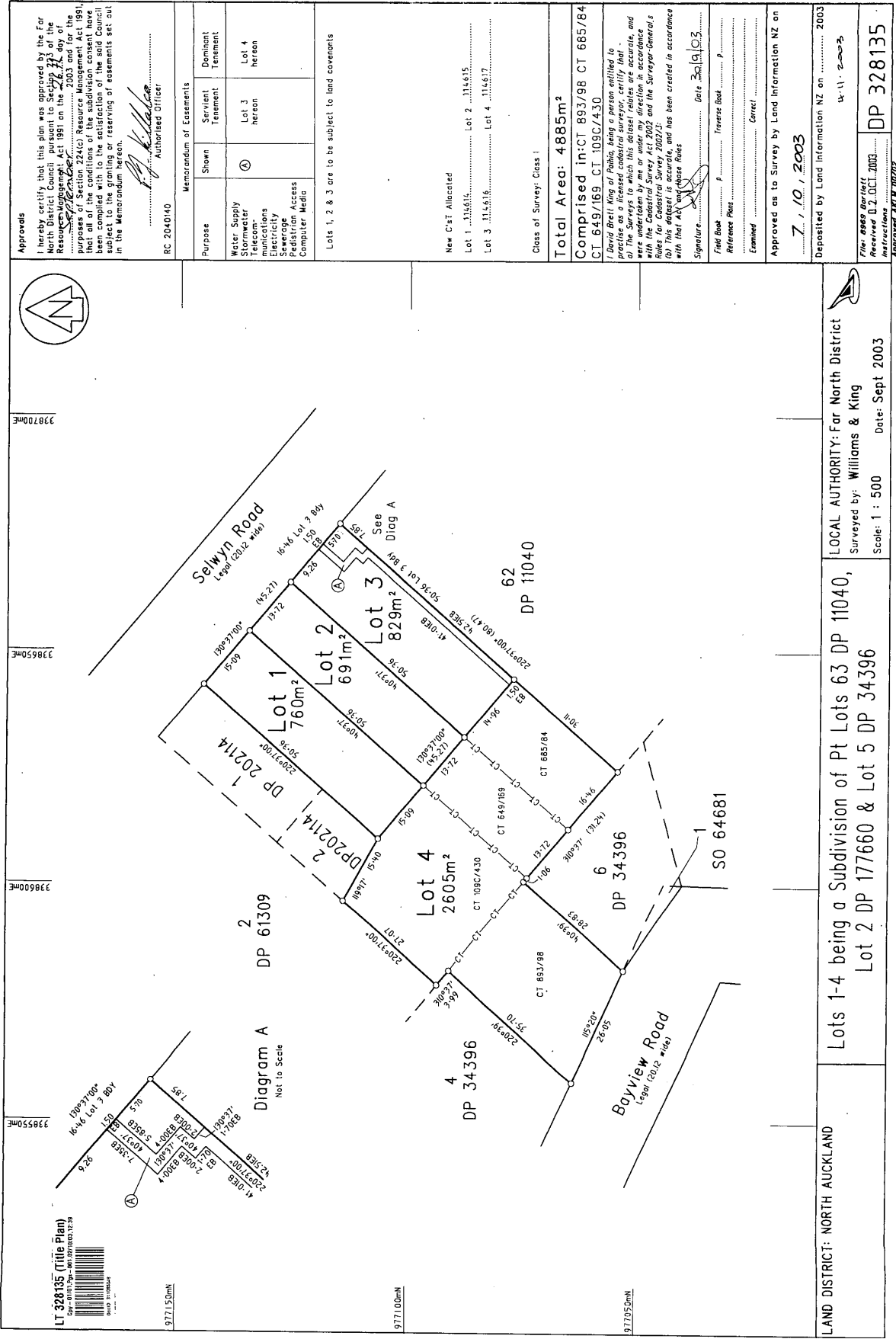
The easements created by Easement Instrument 5785479.5 are subject to Section 243 (a) Resource Management Act 1991

Land Covenant in Transfer 5830709.1 - 9.12.2003 at 9:00 am

Fencing Covenant in Transfer 5830709.1 - 9.12.2003 at 9:00 am

5959168.1 Variation of the land covenant specified in Transfer 5830709.1 - 7.4.2004 at 9:00 am

13032559.3 Mortgage to HP Capital Limited - 30.8.2024 at 5:19 pm



Approvals
 I hereby certify that this plan was approved by the Far North District Council pursuant to Section 223 of the Resource Management Act 1991 on the 26th day of September 2003 and for the purposes of the Resource Management Act 1991 that all of the conditions of the subdivision consent have been complied with to the satisfaction of the said Council subject to the granting or reserving of easements set out in the Memorandum hereon.

[Signature]
 Authorised Officer

RC 204-D140

Memorandum of Easements		Dominant Tenement
Purpose	Shown	Servient Tenement
Water Supply Stormwater Telecom- munications Electricity Sewerage Pedestrian Access Computer Media	(A)	Lot 3 hereon
		Lot 4 hereon

Lots 1, 2 & 3 are to be subject to land coverants

New C&T Allocated
 Lot 1 114616 Lot 2 114615
 Lot 3 114616 Lot 4 114617

Class of Survey: Class 1

Total Area: 4885m²
Comprised in: CT 893/98 CT 685/84
CT 649/169 CT 109C/430

[Signature]
 Date 26/9/03

Field Book Traverse Book
 Reference Plans
 Examined Correct

Approved as to Survey by Land Information NZ on
7.10.2003

Deposited by Land Information NZ on 2003
 14-11-2003

File: 8889 Bartlett
 Received: 12 OCT 2003 DP 328135
 Instructions

APPROVED PLAN 2003

LAND DISTRICT: NORTH AUCKLAND

Lots 1-4 being a Subdivision of Pt Lots 63 DP 11040,
 Lot 2 DP 177660 & Lot 5 DP 34396

LOCAL AUTHORITY: Far North District
 Surveyed by: Williams & King
 Scale: 1 : 500 Date: Sept 2003

From: Ishan Koshatwar <Ishan.Koshatwar@fndc.govt.nz>

Sent: Thursday, October 17, 2024 3:27:22 PM

To: Vance Hodgson <vance@hpcplanning.co.nz>

Cc: Kyron Gosse <kyrongosse@gmail.com>

Subject: Re: CDM-2025-53, 34 Selwyn Road, Paihia - FNDC Rooding & Development Engineer comments

Hi Vance,

Apologies for not getting back to you sooner, I have been off sick lately. I understand your concerns regarding the avoidance of costly assessments and works for the proposed temporary activity. I appreciate the additional research and data provided by Kyron. As long as you comply with the Council's District Plan rules, particularly Chapter 15 - Transportation of the ODP, and the Engineering Standards, your proposal remains valid.

However, please note that I am constrained by certain limitations and will not be able to provide further advice on this matter. If you feel the final proposal aligns well with the Council's requirements, you can proceed and if not I would suggest engaging a suitably qualified engineer.

Have a great evening!

Best regards,



Ishan Koshatwar

Resource Consents Engineer - Resource Consents - Engineering

M 0273026261 | P +6494015357 | Ishan.Koshatwar@fndc.govt.nz

Te Kaunihera o Te Hiku o te Ika | Far North District Council

Pokapū Kōrero 24-hāora | 24-hour Contact Centre 0800 920 029

fndc.govt.nz





From: Vance Hodgson <vance@hpcplanning.co.nz>

Sent: Monday, October 14, 2024 8:21 AM

To: Ishan Koshatwar <Ishan.Koshatwar@fndc.govt.nz>; kyrongosse@gmail.com <kyrongosse@gmail.com>

Cc: Yuna Zhou <Yuna.Zhou@fndc.govt.nz>

Subject: RE: CDM-2025-53, 34 Selwyn Road, Paihia - FNDC Roding & Development Engineer comments

Hi Ishan

We are keen to talk further on the TIA and crossing and hope to avoid costly assessments and works for what is a temporary activity. The intent is very much to avoid any adverse effects and add some activity into the town centre for a site that will otherwise sit vacant.

Kyron has done some further work around the trip generation anticipated and assessed as follows.

Could we follow up with another discussion with you?

Regards

Vance



Vance Hodgson

**HODGSON PLANNING
CONSULTANTS**

Planning Consultant / Director

**027 555 5450 / 09 235 5400 (09 235
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This development proposal includes 5 tourist cabins, 5 overnight parking spots for self-contained campervans, and 6 all-day parking spots. I have calculated the expected vehicle trips based on standard trip generation rates widely used in New Zealand and supported by transport-related research reports. The following analysis explains the logic behind the estimated vehicle trips and demonstrates how the traffic generated will remain well within acceptable limits for the area.

1. Tourist Cabins

The 5 tourist cabins will generate an estimated 8 vehicle trips per cabin per day. This figure is derived from typical trip generation data for tourist accommodations, which accounts for guests arriving, departing, and making additional trips during their stay for local activities such as dining or sightseeing. According to Research Report 453 from the New Zealand Transport Agency (NZTA), accommodations like motels or tourist cabins generate between 6 to 10 vehicle trips per unit per day depending on the nature of the accommodation. Given the location of the cabins and the walking accessibility to town we have gone with the lower number. But even at the higher number it still puts us well within the permitted activity for the zoning.

Total trips from cabins: 5 cabins × 6 trips = 30 vehicle trips per day.

2. Overnight Parking Spots for Self-Contained Campervans

The 5 parking spots for self-contained campervans are expected to generate approximately 4 vehicle trips per spot per day. These trips include the arrival and departure of the campervans, and allowing for 2 vehicles per spot per day. Data suggests that campervans generally generate fewer trips than other forms of tourist accommodation, given that they often remain parked overnight without frequent vehicle movements. The nature of these parks are that once a campervan is there, they will be staying for at least 12-24 hours. Those campervans looking for short term car parking will use the FNHL carpark or street car parks nearby.

Total trips from campervans: 5 spots × 4 trips = 20 vehicle trips per day.

3. All-Day Parking Spots

The 6 all-day parking spots will be used by visitors arriving in the morning and departing in the evening, generating 2 vehicle trips per day per spot. This is based on the assumption that these spots will serve visitors who are looking for all day parking options in close proximity to the Paihia wharf. These are typically fishermen heading out at 8am and returning around 5pm. Or families heading out on cruises throughout the Bay. These are designed as single price all day parking and cars here will not be making multiple trips during their stay.

Total trips from all-day parking: 6 spots × 2 trips = 12 vehicle trips per day.

Total Estimated Traffic Movements

By adding the expected vehicle trips from each component of the development, we estimate a total of 67 vehicle trips per day:

Tourist cabins: 30 trips

Campervan spots: 20 trips

All-day parking spots: 12 trips

Total daily vehicle trips: $30 + 20 + 12 = 62$ vehicle trips per day.

This represents 31% of the total trips allowed as a permitted activity, therefore we have plenty of room for a margin of error.

Conclusion

The estimated total of 62 vehicle trips per day is consistent with traffic generated by similar small-scale tourist accommodations in New Zealand. These estimates are in line with standards used in traffic impact assessments and comply with the Far North District Council's guidelines for traffic intensity thresholds. Given the commercial nature of the area and the anticipated low volume of additional traffic, this development is not expected to place undue strain on the local road network or require significant traffic mitigation measures.

The calculations and logic presented above are supported by established traffic generation data sources, including NZTA Research Report 453, which provides trip generation data specifically for tourist accommodation developments. Furthermore, the trip generation thresholds established by Far North District Council ensure that developments of this nature can be managed without the need for extensive traffic management interventions.

References/Attached:

NZTA Research Report 453: <https://www.nzta.govt.nz/resources/research/reports/453/>

Far North District Council District Plan:

Far North Maps - Zoning and traffic considerations

District Plan Review – Trip Threshold in TRAN Table 11

From: Ishan Koshatwar <Ishan.Koshatwar@fndc.govt.nz>

Sent: Thursday, 10 October 2024 3:22 pm

To: Vance Hodgson <vance@hpcplanning.co.nz>; kyrongosse@gmail.com

Cc: Yuna Zhou <Yuna.Zhou@fndc.govt.nz>

Subject: CDM-2025-53, 34 Selwyn Road, Paihia - FNDC Rooding & Development Engineer comments

Hi Vance,

I acknowledge that there are no offsite effects being exacerbated by the development. As mentioned earlier, considering the existing flooding situation on the downstream property, a general assessment of the proposed development and its effects needs to be included in the Resource Consent application. In terms of stormwater management, runoff from the new development and impermeable surfaces needs to be at least restricted to 80% of pre-development levels to ensure compliance with the Far North District Plan.

As discussed, I have liaised internally with the relevant departments and you can find their comments on the above-mentioned proposal below.

FNDC Rooding Input:

Although your proposal is situated within the Commercial Zone under the operative district plan, we would appreciate it if you could provide a traffic assessment with your resource consent application. It appears that the site will host a variety of mixed-use activities, each with different traffic generation rates. Therefore, a detailed breakdown of traffic generation for each activity would be beneficial. Additionally, if the on-site activities depend on on-street parking, we request that you conduct a parking survey to help us better understand parking availability. According to the plans, some of the parallel parking spaces on Selwyn Road may breach the Land Transport Act, as the vehicle crossing will be within 1 meter of parked vehicles.

Development Engineer (Infrastructure Strategy) Input:

There are no major concerns with the stormwater connection to the public reticulated system. The existing wastewater line appears to be adequate, with no significant issues identified at this stage. The proposed site is well within the area of benefit. However, given the nature of the terrain, water booster pumps may be required for the cabin located at high elevations. All designs must comply with the current Engineering Standards.

Kind regards,



Ishan Koshatwar

Resource Consents Engineer - Resource
Consents - Engineering

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**Te Kaunihera o Te Hiku o te Ika | Far North District
Council**

**Pokapū Kōrero 24-hāora | 24-hour Contact
Centre 0800 920 029**

fndc.govt.nz



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Te Ao Māori Framework

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TE KŌWHIRI



WHANAKE TAHI



KŌKIRI TAHI



PROJECT
DEVELOPMENT STAGE 1

FOR
PAIHIA TERRACES Ltd.

SITE ADDRESS
**30-34 SELWYN ROAD,
 PAIHIA
 BAY OF ISLANDS
 FAR NORTH DISTRICT COUNCIL**

ISSUE
SCHEME PRELIM RC

DATE
18 DEC 2024

PROJECT REF
24-059



Layout	Rev	Sheet Name
Plans		
A01		COVER SHEET
A02		EXISTING SITE PLAN
A03		PROPOSED SITE PLAN
A04		PROPOSED GWE STORMWATER DRAINAGE
A05		PARKING
A06		EARTHWORKS
A07		SITE OVERVIEW
A08		ELEVATIONS
A09		ELEVATIONS
A10		VISUALS 1
A11		VISUALS 2
A12		VISUALS 3
A13		WHITE MODEL



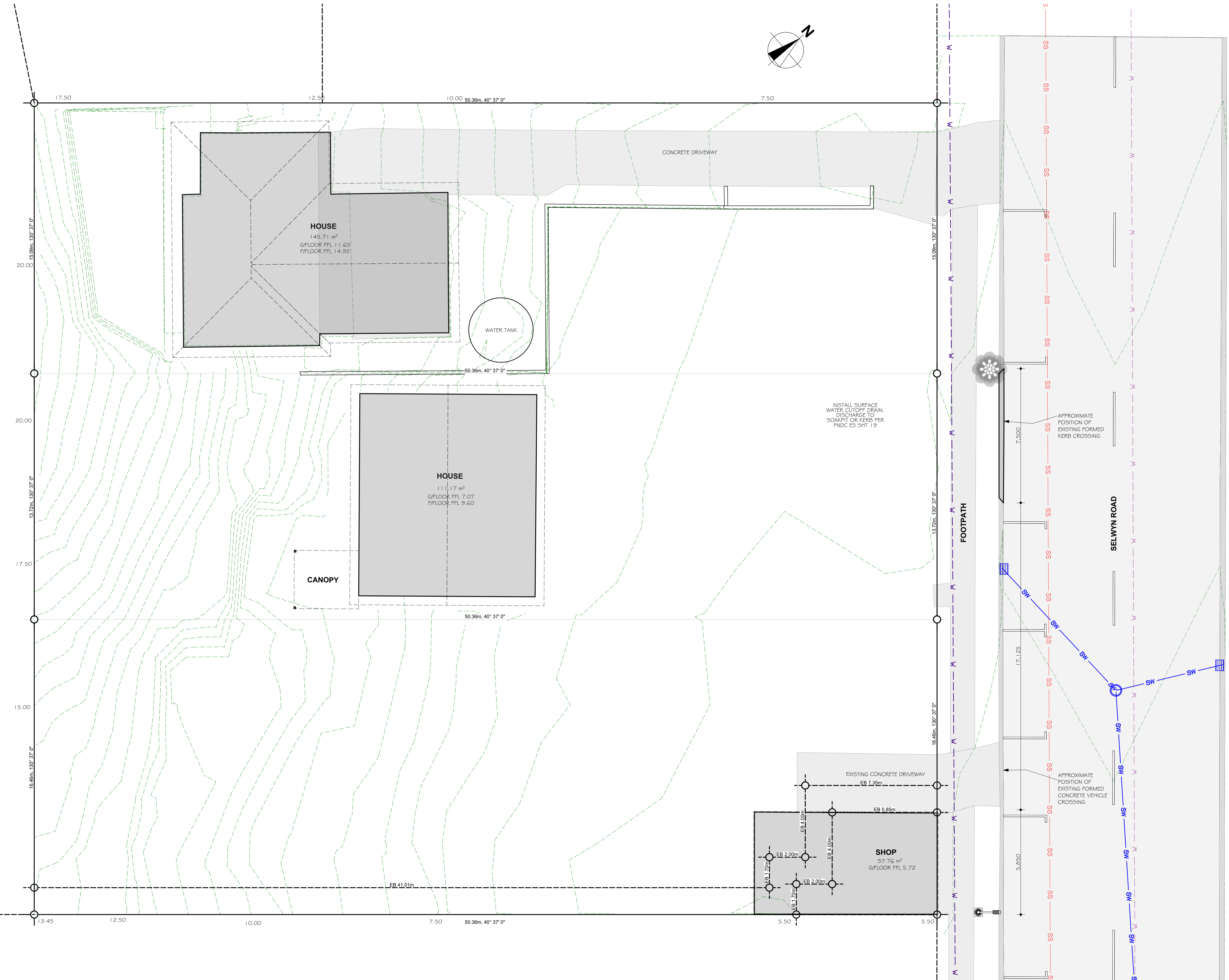
LEGAL DESCRIPTION & SITE DETAILS

No. 30, 32 & 34 SELWYN STREET,
PAIHIA
TERRITORIAL AUTHORITY-
FAR NORTH DISTRICT COUNCIL

LOT 1, 2 & 3 DP 328135
SITE AREA 2280m² (COMBINED)

PLANNING - FAR NORTH DISTRICT
OPERATIVE DISTRICT PLAN
ZONE PAIHIA COMMERCIAL
SUB ZONE AREA AS
ZONE OVERLAY PEDESTRIAN FRONTAGE

PROPOSED DISTRICT PLAN, NOTIFIED
ZONE MIXED USE
SUB ZONE AREA AS
ZONE OVERLAY PEDESTRIAN FRONTAGE



EXISTING SITE PLAN
1:100

RevID	CHD	Revision	Date

CLIENT
PAIHIA TERRACES Ltd.

ADDRESS
**30-34 SELWYN ROAD,
PAIHIA
BAY OF ISLANDS
FAR NORTH DISTRICT COUNCIL**

PROJECT
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EXISTING SITE PLAN

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DATE 18 DEC 2024	SHT FORMAT A1	
JOB REF 24-059	TOTAL SHTS 13	
	SHEET	
		REVISION

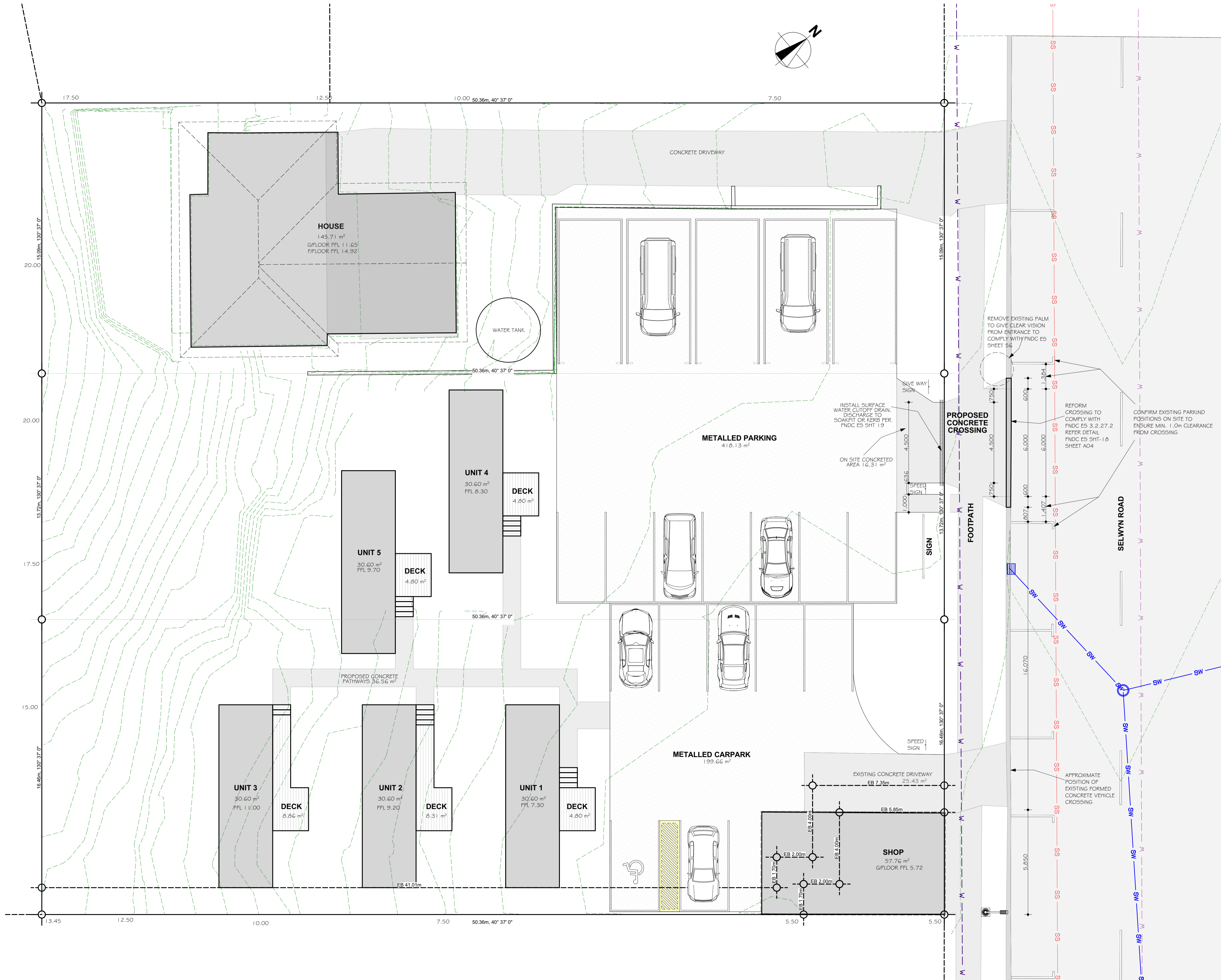
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TERRITORIAL AUTHORITY-
FAR NORTH DISTRICT COUNCIL

LOT 1, 2 & 3 DP 328135
SITE AREA 2280m² (COMBINED)

PLANNING - FAR NORTH DISTRICT
OPERATIVE DISTRICT PLAN
ZONE PAIHIA COMMERCIAL
SUB ZONE AREA A3
ZONE OVERLAY PEDESTRIAN FRONTAGE

PROPOSED DISTRICT PLAN, NOTIFIED
ZONE MIXED USE
SUB ZONE AREA A3
ZONE OVERLAY PEDESTRIAN FRONTAGE



RevID	CHD	Revision	Date

CLIENT
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CRAIG	AS SHOWN		
DATE	SHT FORMAT		
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JOB REF	TOTAL SHTS		
24-059	13		

PROPOSED SITE PLAN
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RevID	CHD	Revision	Date
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PROJECT DEVELOPMENT STAGE 1			



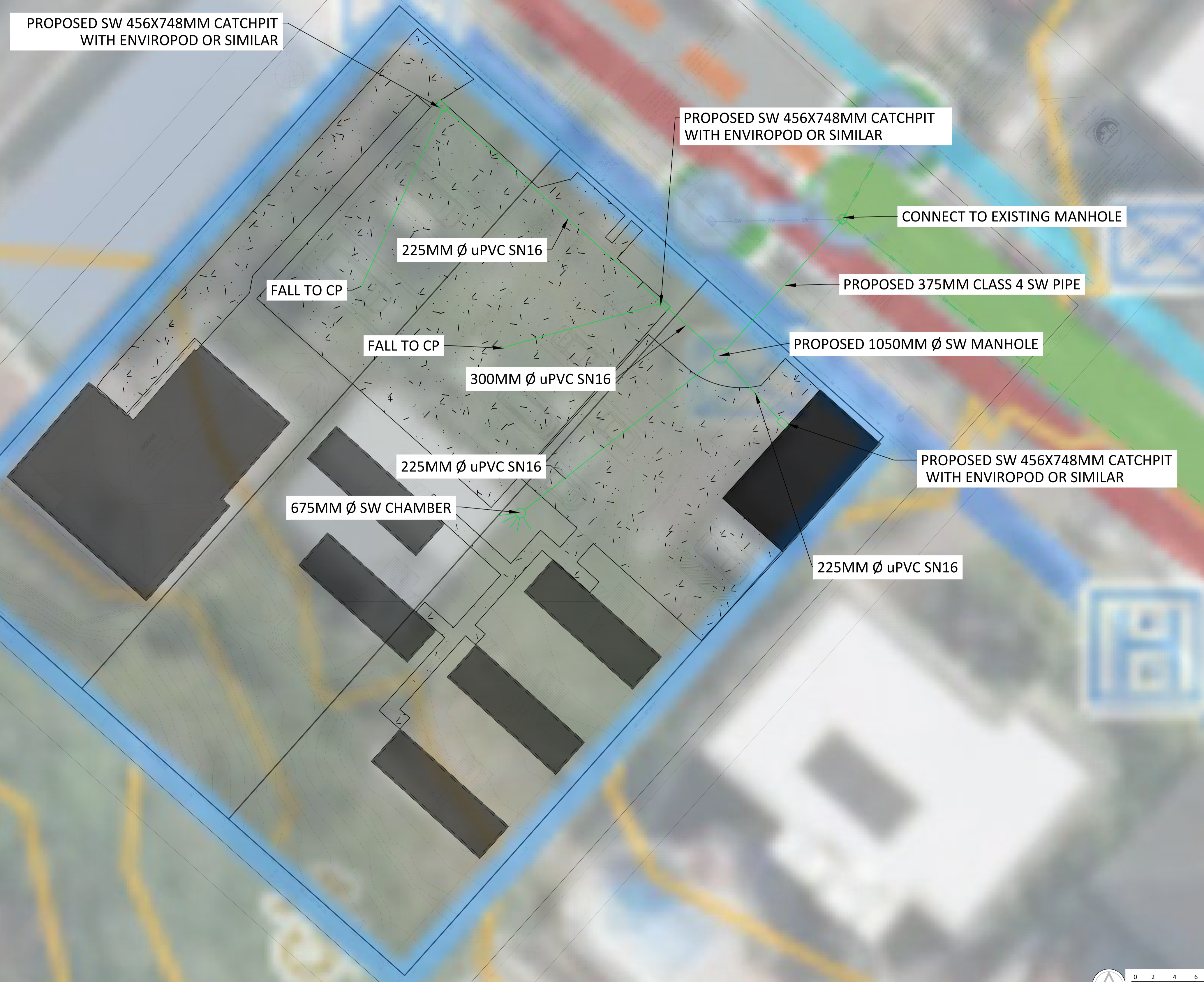
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**PROPOSED GWE STORMWATER
DRAINAGE**

DRAWN CRAIG	SCALE AS SHOWN	A04
DATE 18 DEC 2024	SHEET FORMAT A1	
JOB REF 24-059	TOTAL SHEETS 13	SHEET REVISION



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REV	AMENDMENT	CAD	ENG	APPD	DATE

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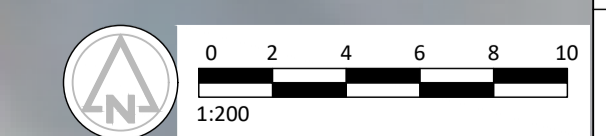
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AUCKLAND 0622
P:+64 9 445 8338
www.gwe.co.nz

PROJECT ADDRESS:
30, 32, 34 SELWYN ROAD, PAIHIA

DRAWING TITLE:
STORMWATER SITE PLAN

CLIENT NAME:
PAIHIA TERRACES LTD.

SCALE: 1:150	A1
PROJECT No: J2571-1	DRAWING No: 400
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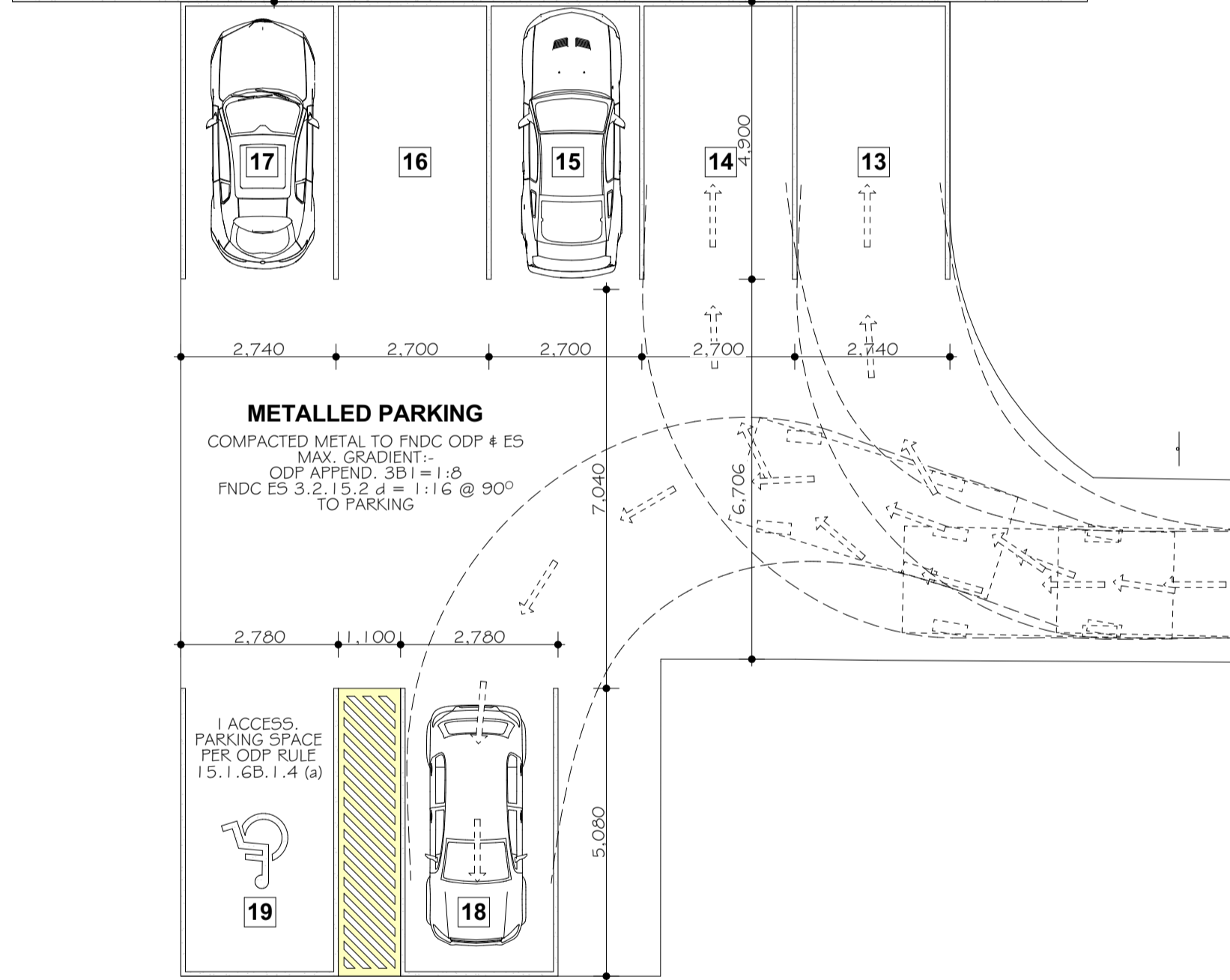
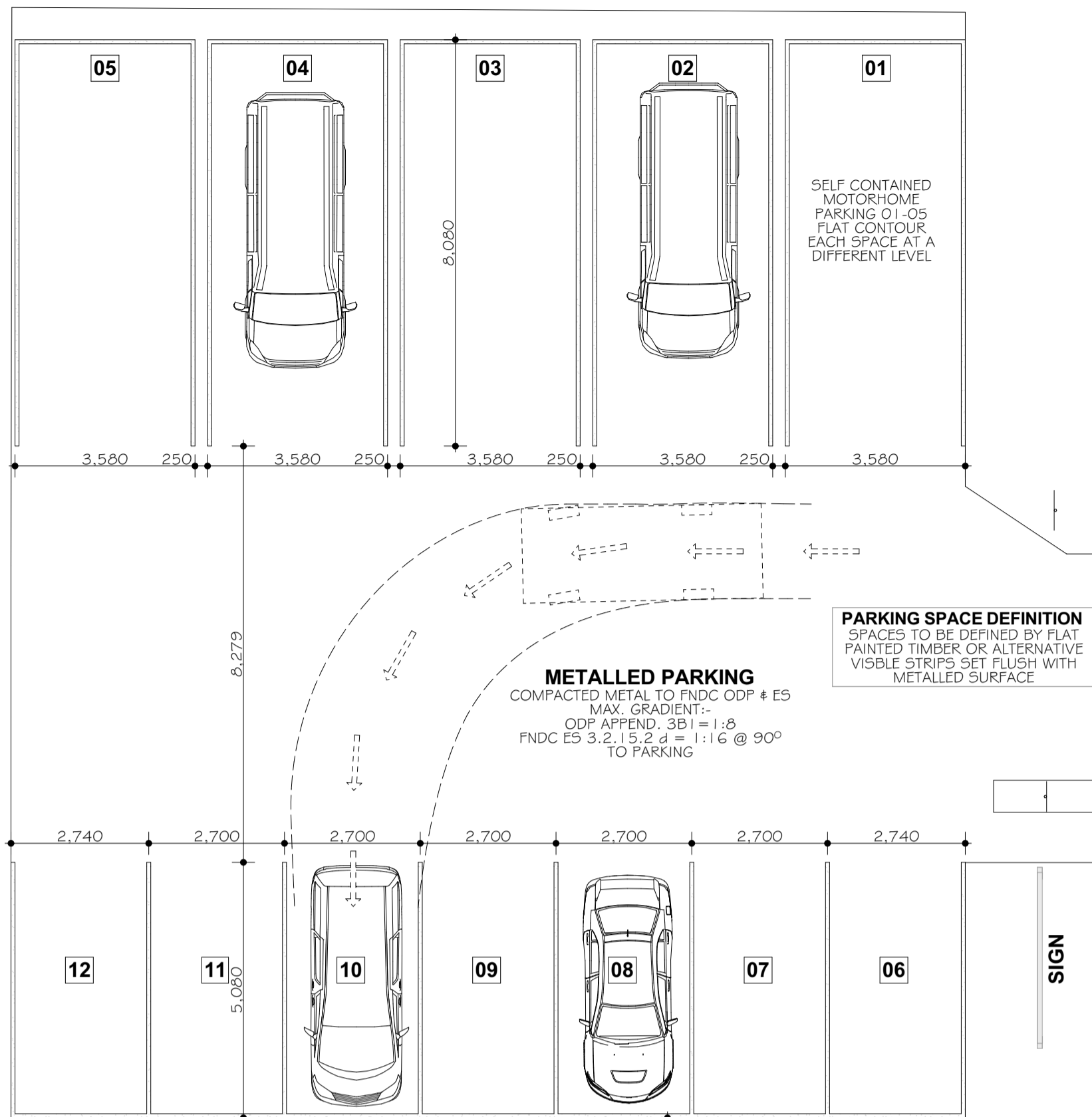
APPENDIX 3D: MANOEUVRING & PARKING SPACE DIMENSIONS

(Reference: Part 3 District Wide Provisions, Section 15.1 Traffic, Parking and Access)
(all dimensions in metres)

Parking Angle	Width of Parking Space	Kerb Overhang	Depth of Parking Space	Manoeuvring Spaces	Total Depth One Row	Total Depth Two Rows
90° Regular Users ⁽¹⁾	2.4 ⁽²⁾	1.0	4.9	7.1	12.9	16.9
	2.5	1.0	4.9	6.7	11.6	16.5
	2.6	1.0	4.9	6.3	11.2	16.1
90° Casual Users ⁽¹⁾	2.7	1.0	4.9	5.9	10.8	15.7
	2.75	1.0	4.9	5.9	10.8	15.7
	2.7	1.0	4.9	6.6	11.6	16.5
75°	2.4 ⁽²⁾	1.0	5.2	6.5	11.7	16.9
	2.5	1.0	5.2	6.0	11.2	16.4
	2.6	1.0	5.2	5.7	10.9	16.1
60°	2.7	1.0	5.2	5.0	10.2	15.4
	2.75	1.0	5.2	4.3	9.5	14.7
	2.4 ⁽²⁾	1.0	5.2	4.6	9.8	15.0
45°	2.5	1.0	5.2	4.1	9.3	14.5
	2.6	1.0	5.2	3.5	8.7	13.9
	2.7	1.0	5.2	3.3	8.5	13.7
30°	2.7	0.8	4.9	3.2	8.4	13.6
	2.4 ⁽²⁾	0.8	4.9	2.9	7.8	12.7
	2.5	0.8	4.9	2.7	7.6	12.5
Parallel	2.6	0.8	4.9	2.5	7.4	12.3
	2.7	0.8	4.9	2.4	7.3	12.2
	2.75	0.8	4.9	2.3	7.2	12.1
Parallel	5.9	0.4	2.5	3.6	6.1	8.6
	6.1	0.4	2.5	3.3	5.8	8.3
	6.3	0.4	2.5	3.0	5.5	8.0

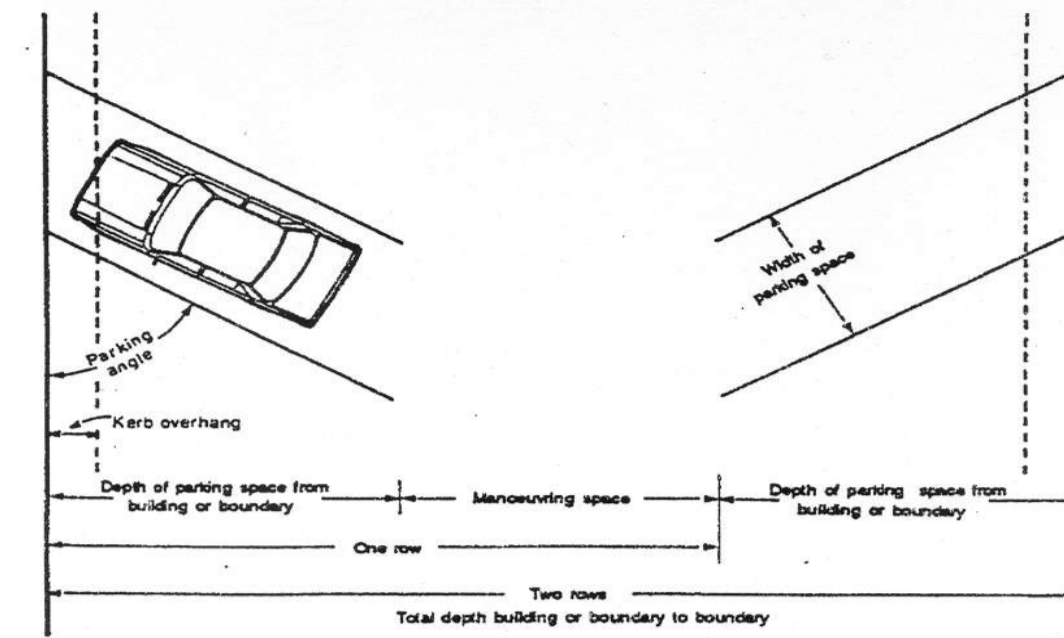
- Regular users are people whose regular use gives them a familiarity with the building that permits smaller safe clearances between vehicles and parts of buildings. Casual users are people (usually short-term visitors) who would not be familiar with the building layout.
 - Stall widths of 2.4m should generally only be used where users are familiar with the car park. This stall width does not meet the requirements of the Building Code for Casual Users.
- Notes:**
- Minimum aisle widths are 3.6m for a one-way aisle, and 5.5m for a two-way aisle. Where an aisle serves more than 50 spaces, it should be designed as a circulation route in which case the minimum width for a two-way aisle increases to 6.5m. Note that the Building Code requires an extra 0.8m width where pedestrians use a vehicle circulation route.
 - Stall widths shall be increased by 0.3m where they abut obstructions such as columns or walls.
 - All overhang areas shall be kept clear of objects greater than 150mm in height.
 - Where parallel end spaces have direct access through the end of the stall the length of the stall may be reduced to 5.4m.
 - One-way traffic is assumed for angle spaces.
 - Car park height shall be at least 2.3m over the full area of the space, except where special provision is made to divert over height vehicles, in which case the minimum height may be reduced to 2.1m.
 - Note that the Building Code may require car park spaces to be provided for people with disabilities. Details of the requirements for these spaces may be found in NZS 4121.
 - Linear interpolation is permitted for stall width, parking angle and aisle width.
 - Car park spaces which comply with the preferred design envelope shown below are deemed to comply with the dimensions in this Appendix.

Opp Plan - Appendix-3-Traffic-and-parking

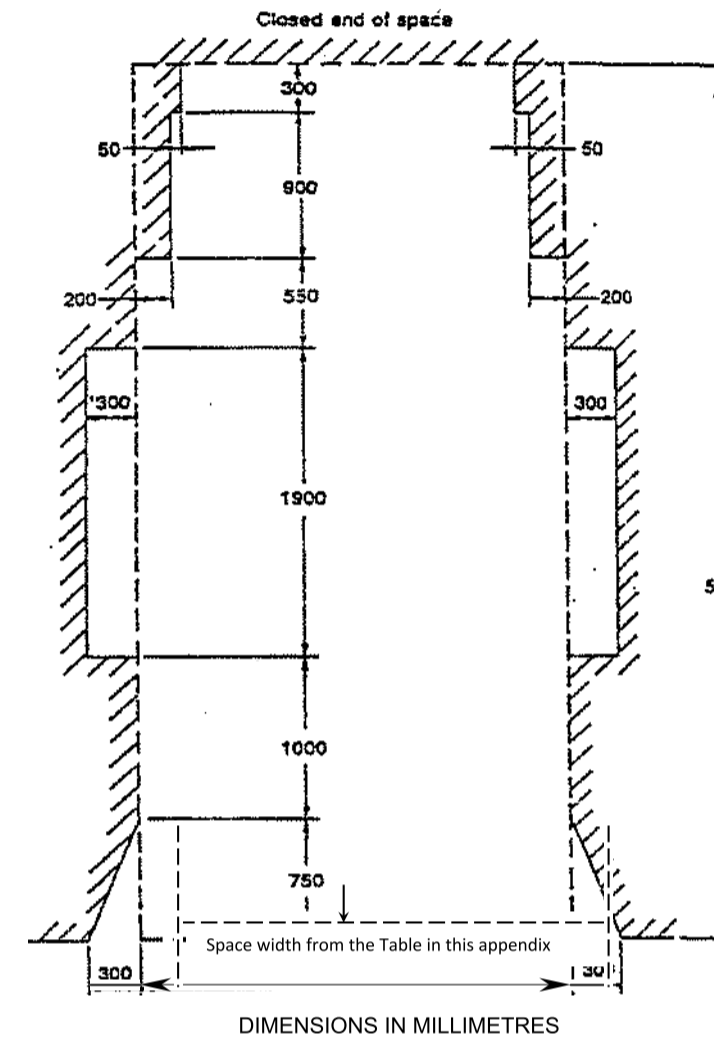


PARKING & MANEUVERING
1:100

Manoeuvring and Parking Space Dimensions:

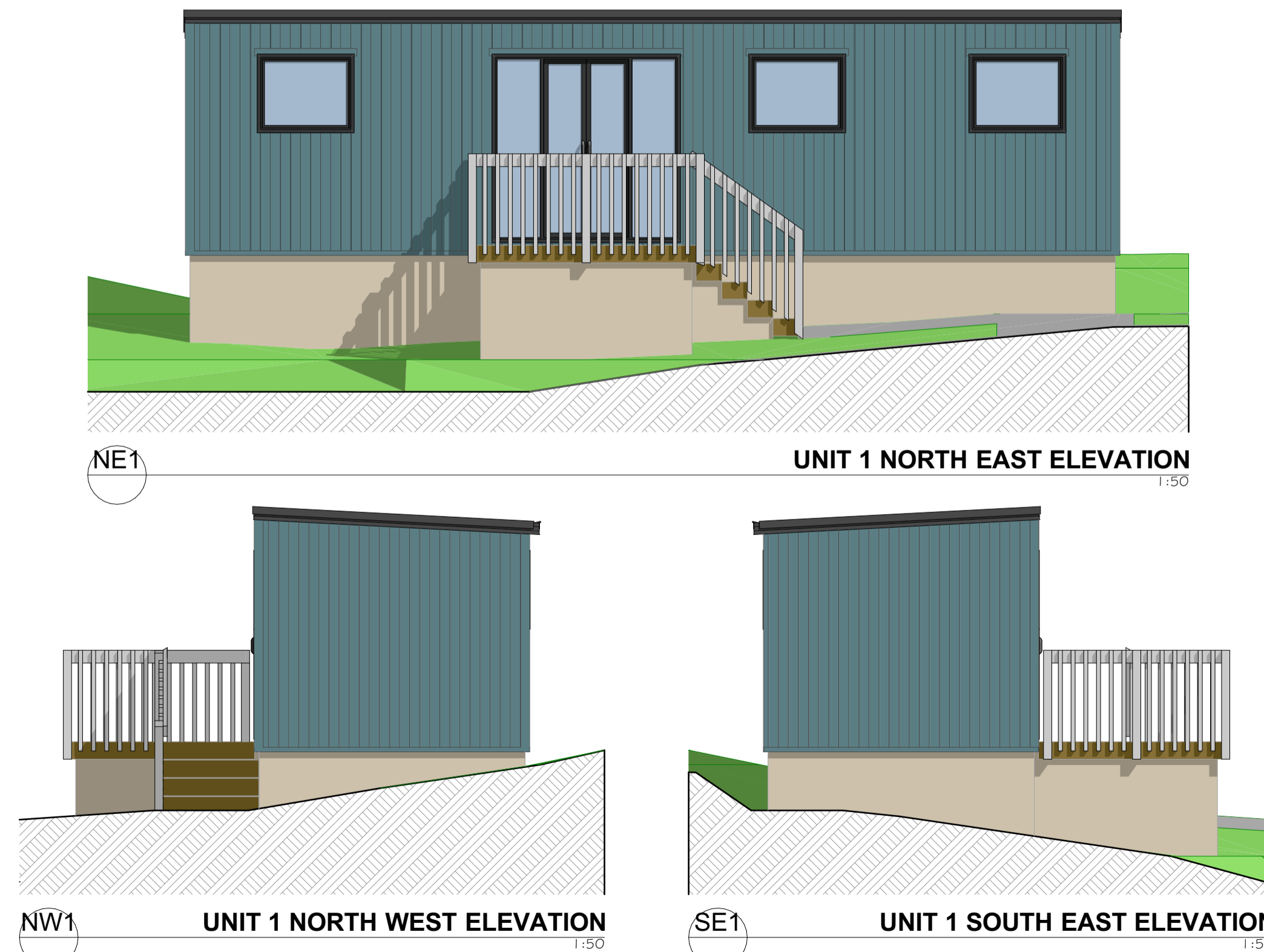


Preferred Design Envelope around Parked Vehicle to be kept clear of Columns, Walls and Obstructions:

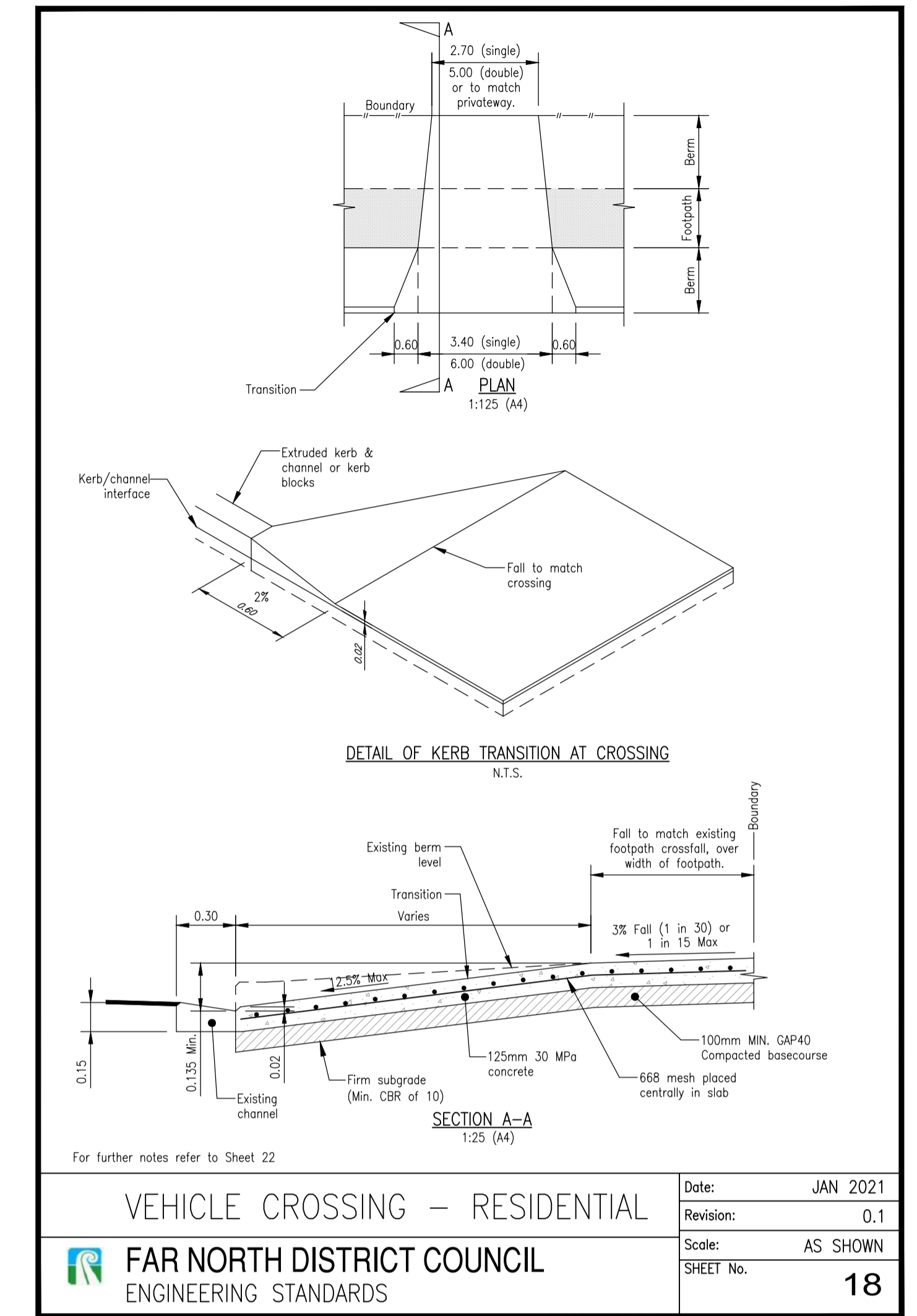


Note: The preferred design envelope provides for structural elements to be clear of all four side doors whereas the standard provides for the opening of the front door only (when nose in).

Opp Plan - Appendix-3-Traffic-and-parking



Sheet 18 Vehicle Crossing - Residential



VEHICLE CROSSING – RESIDENTIAL		Date: JAN 2021
FAR NORTH DISTRICT COUNCIL ENGINEERING STANDARDS		Revision: 0.1
		Scale: AS SHOWN
		SHEET No. 18

FNDC ES SHT 18

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PROJECT DEVELOPMENT STAGE 1			

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PARKING

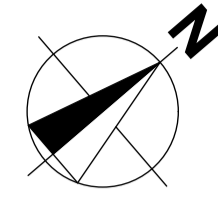
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DATE 18 DEC 2024	SHT FORMAT A1	
JOB REF 24-059	TOTAL SHTS 13	



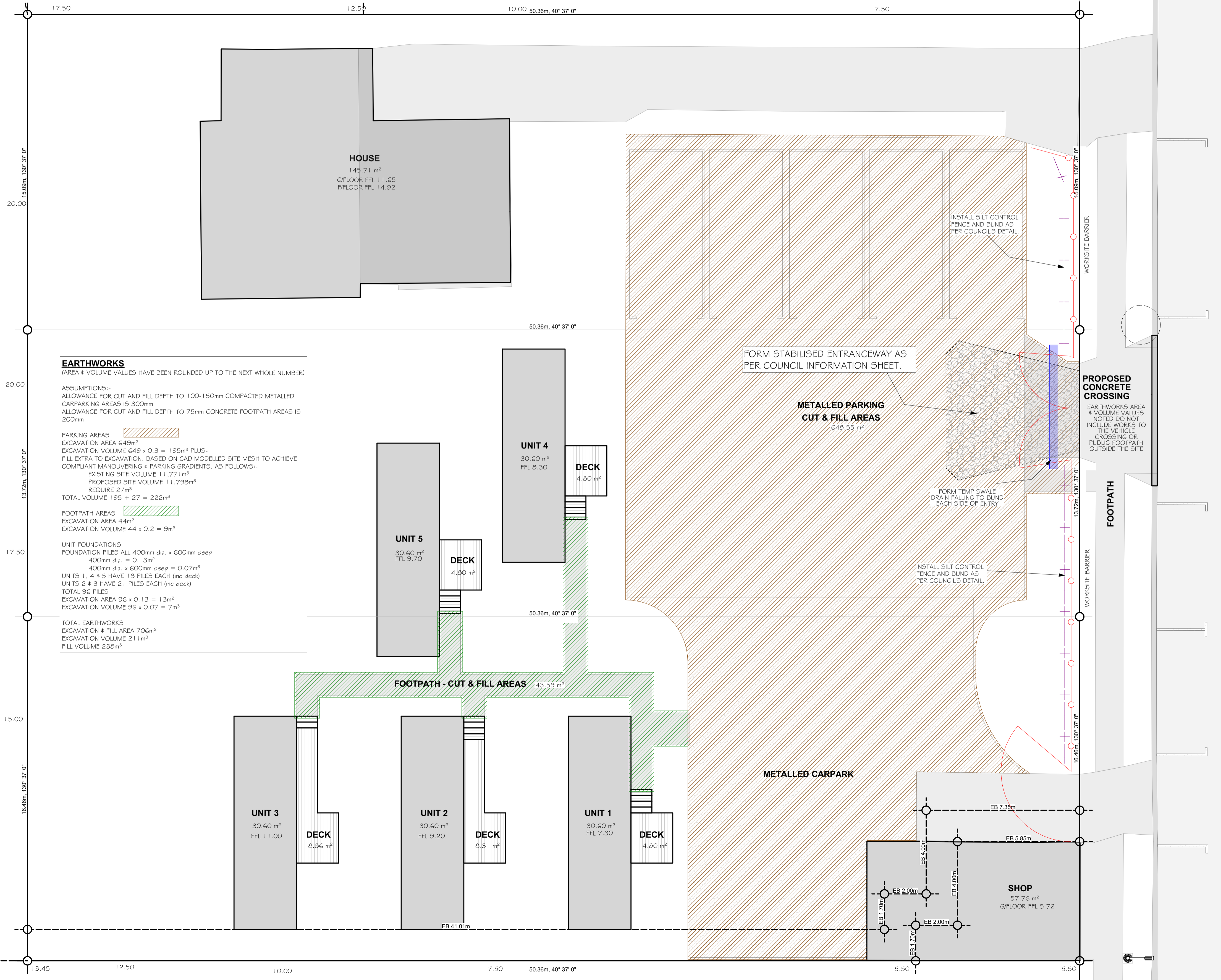
SPECIFICATIONS	
DIMENSIONS	2400(W) x 2000(H)MM
UOM	Each
UV RESISTANT	Yes
MESH TYPE	Anti-Climb
FINISH	Galvanneal
PRIMARY MATERIAL	Steel
PACK SIZE	1
PALLET SIZE	54
TITLE AVAILABLE	2000 Series
STANDARDS/RATING/KEY	Australian Standards AS4887
CUSTOM PRINTED	Printed Fence Fabric available
RELATED PRODUCTS	Handrails Post Closures Channel Fence Fabric
OTHER INFORMATION	Temp Fencing 2000 Series Economy/Precast Panels
	Suitable for medium term projects. Must be installed in accordance with the installation guide.

WORK SITE BARRIER

INSTALL 2m HIGH TEMPORARY CONSTRUCTION SITE FENCING TO NZBC F5/AS 1 (50:50 GALV CHAIN LINK NETTING). POTENTIAL SPECIFIC HAZARDS ON SITE MAY REQUIRE BARRIERS DURING CONSTRUCTION AND ARE TO BE PUT IN PLACE AS PER NZBC F5/AS 1.



D-02 WORKSITE BARRIER



EARTHWORKS
(AREA & VOLUME VALUES HAVE BEEN ROUNDED UP TO THE NEXT WHOLE NUMBER)

ASSUMPTIONS:-
ALLOWANCE FOR CUT AND FILL DEPTH TO 100-150mm COMPACTED METALLED CARPARKING AREAS IS 300mm
ALLOWANCE FOR CUT AND FILL DEPTH TO 75mm CONCRETE FOOTPATH AREAS IS 200mm

PARKING AREAS [diagonal hatching]
EXCAVATION AREA 649m²
EXCAVATION VOLUME 649 x 0.3 = 195m³ PLUS
FILL EXTRA TO EXCAVATION, BASED ON CAD MODELLED SITE MESH TO ACHIEVE COMPLIANT MANOEUVRING & PARKING GRADIENTS, AS FOLLOWS:-
EXISTING SITE VOLUME: 11,771m³
PROPOSED SITE VOLUME: 11,796m³
REQUIRE 27m³
TOTAL VOLUME 195 + 27 = 222m³

FOOTPATH AREAS [green hatching]
EXCAVATION AREA 44m²
EXCAVATION VOLUME 44 x 0.2 = 9m³

UNIT FOUNDATIONS
FOUNDATION PILES ALL 400mm dia. x 600mm deep
400mm dia. x 0.13m³
400mm dia. x 600mm deep = 0.07m³
UNITS 1, 4 & 5 HAVE 1 Ø PILES EACH (inc deck)
UNITS 2 & 3 HAVE 2 PILES EACH (inc deck)
TOTAL 96 PILES
EXCAVATION AREA 96 x 0.13 = 13m²
EXCAVATION VOLUME 96 x 0.07 = 7m³

TOTAL EARTHWORKS
EXCAVATION & FILL AREA 706m²
EXCAVATION VOLUME 211m³
FILL VOLUME 238m³

Silt fences

Silt fences are useful for containing stockpiles of earth or areas of disturbed soil and clay on site.

It needs to

- be installed in a trench 200mm deep by 100mm wide
- have waratahs or posts hammer-staked at least 400mm deep on the downhill side of the fabric, no more than 2m apart
- be 600mm high above ground, with an additional 200mm of cloth below ground in the trench
- have each end of the fence return up the slope roughly 2m to prevent water going around the edges
- be anchored by backfilling the trench and placing soil on top of the fabric.

How?

Install a silt fence around the downhill side of your site to contain sediment so that when it rains, muddy water stays behind the fence.

Stabilise entrances

Keep vehicles off exposed soil and clay - build a stabilised entranceway with geotextile cloth and large washed aggregate.

How?

- The minimum set-up for your entranceway should be at least:
 - a 150mm thick layer of 65-100mm aggregate
 - 4m wide with a geotextile base
 - long enough for your site with 'wings' (to allow for vehicles cutting corners).
- Don't use materials like sand, crushed concrete or asphalt to make your entranceway.
- Mud should be swept back on to site (not on to the road).

Why?

- A stabilised entranceway prevents vehicles tracking mud and clay on to the road (which is a common source of complaints to council).
- Mud and contaminants can make a road slippery and dangerous. They can be washed into the stormwater system by the rain or create a dust nuisance when it's dry.
- It is your responsibility to ensure that the road is clean of mud and sediment. **Failure to do so can result in a fine or prosecution.**

Be prepared to shift your silt fence and bunds as you move through different stages of your project. Don't use haybales - they don't work.

Building on small sites. Doing it right.

D-03 **SILT CONTROL**

RevID	CHD	Revision	Date

CLIENT
PAIHIA TERRACES Ltd.

ADDRESS
30-34 SELWYN ROAD, PAIHIA, BAY OF ISLANDS, FAR NORTH DISTRICT COUNCIL

PROJECT
DEVELOPMENT STAGE 1

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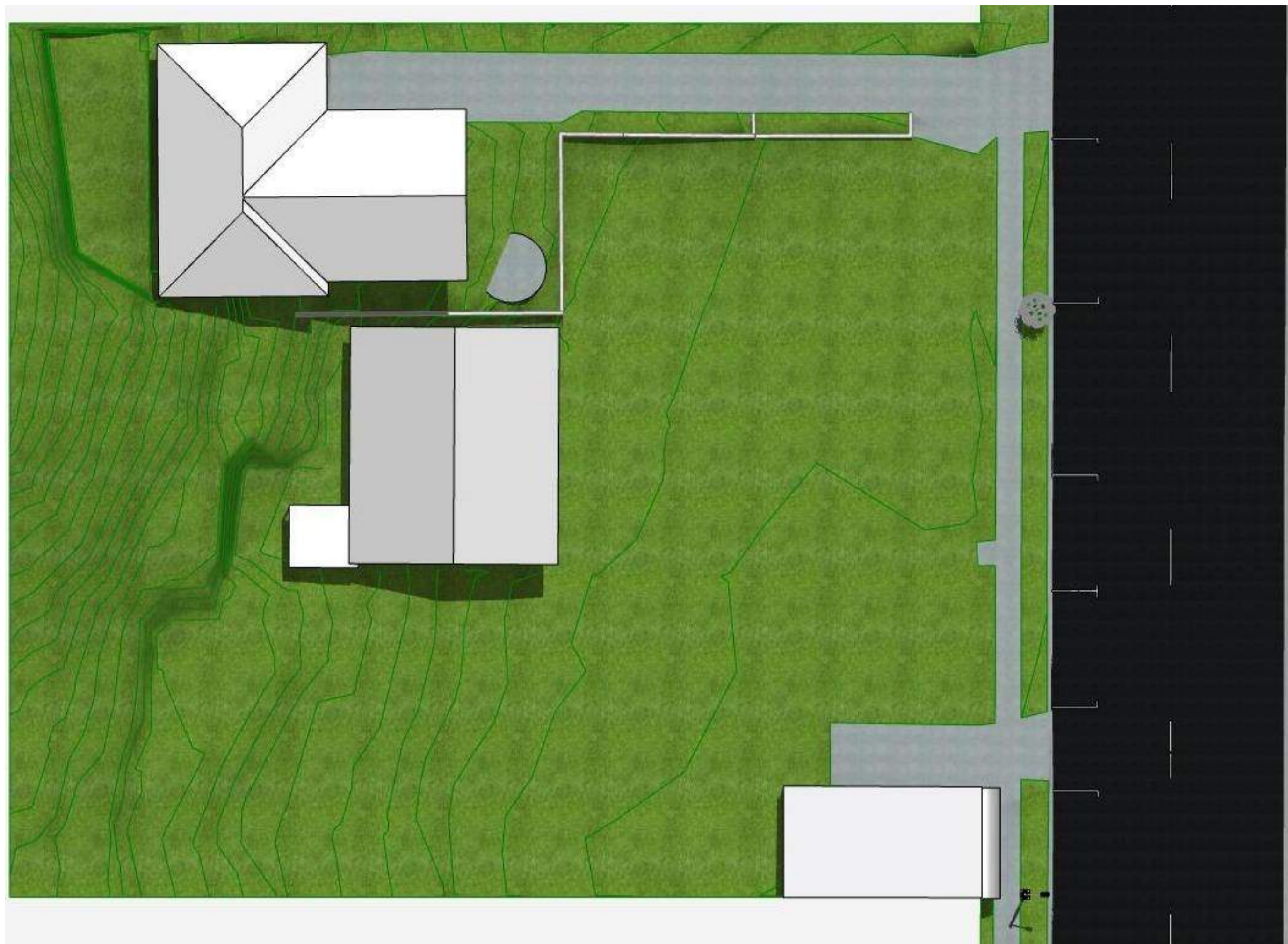
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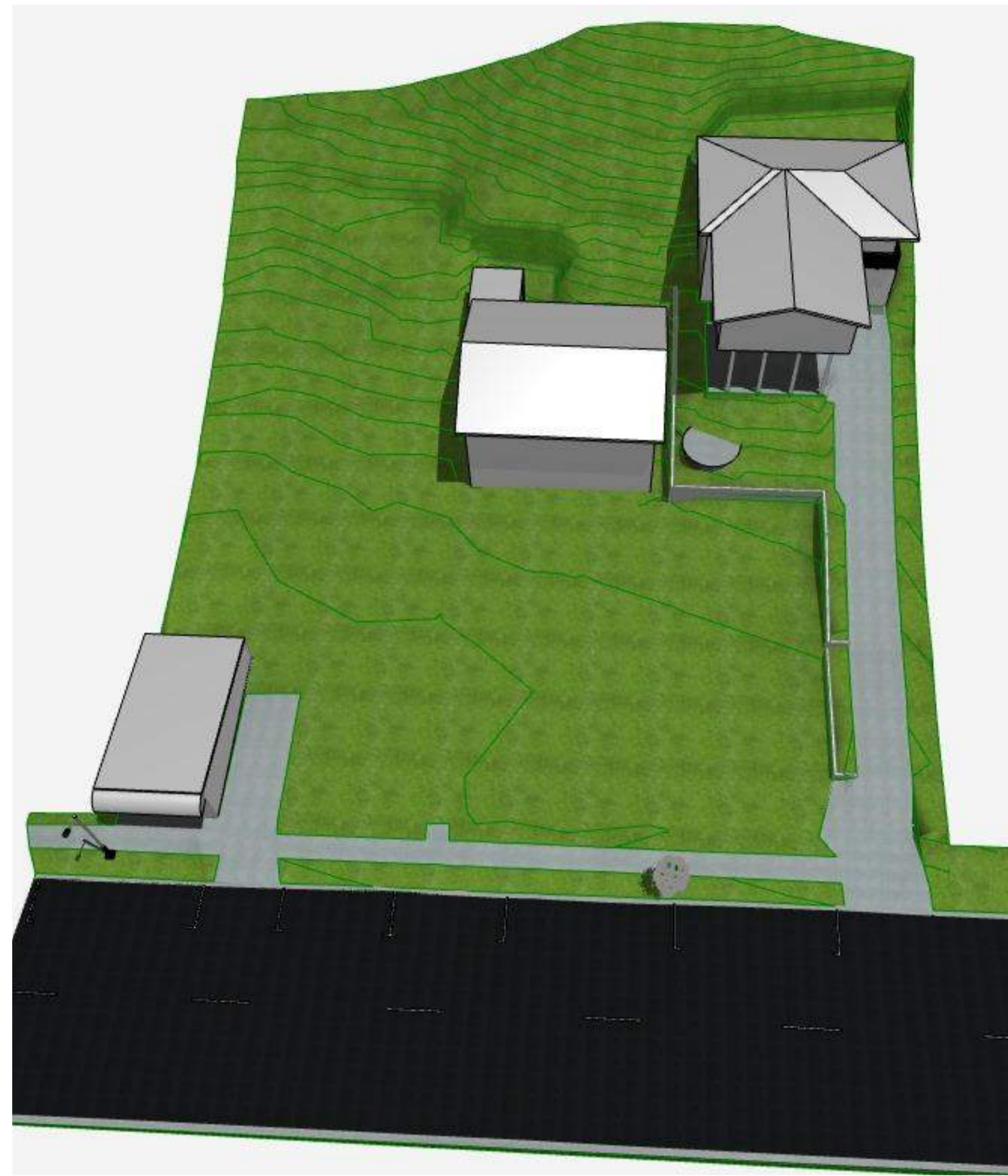
DRAWING NAME
EARTHWORKS

DRAWN	SCALE	A06	SHEET	REVISION
CRAIG	AS SHOWN			
DATE	SHT FORMAT			
18 DEC 2024	A1			
JOB REF	TOTAL SHTS			
24-659	13			

SITE EARTHWORKS PLAN
1:100



EXISTING



EXISTING



PROPOSED



PROPOSED

RevID	CHD	Revision	DATE
CLIENT			
PAIHIA TERRACES Ltd.			
ADDRESS			
30-34 SELWYN ROAD, PAIHIA BAY OF ISLANDS FAR NORTH DISTRICT COUNCIL			
PROJECT			
DEVELOPMENT STAGE 1			



CTM ARCHITECTURAL LTD
18 SWEETCORN PLACE,
PUNESONGE, 2100
PO Box 547
PUNESONGE, 2040
ph/fax: 09 238 4414
web: www.ctm.net.nz
e-mail: info@ctm.net.nz

WORK TO N.Z.B.C. AND RELEVANT CODES
DIMENSIONS SUPERSEDE SCALE
CHECK AND VERIFY ALL LEVELS AND DIMENSIONS
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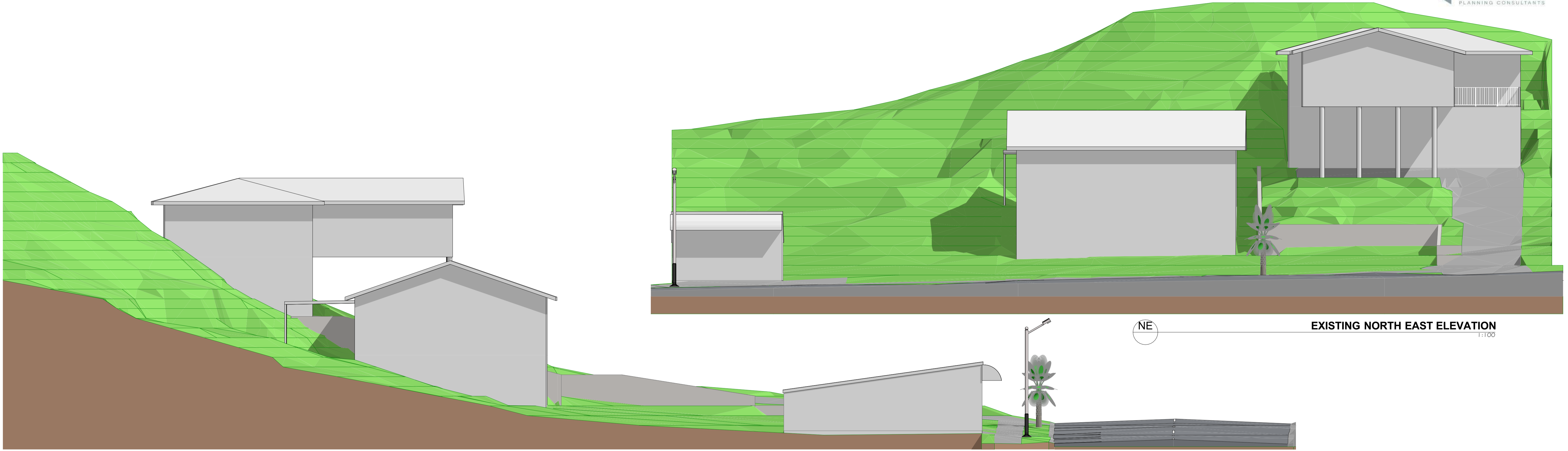
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ISSUE
SCHEME PRELIM RC

DRAWING NAME
SITE OVERVIEW

DRAWN	SCALE	A07
CRAIG	AS SHOWN	
DATE	SHEET FORMAT	
18 DEC 2024	A1	
JOB REF	TOTAL SHEETS	
24-059	13	

SHEET REVISION



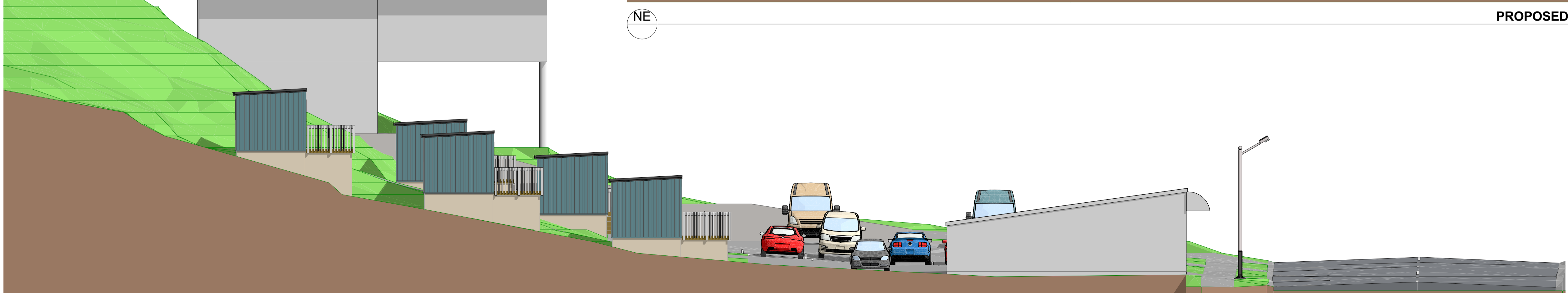
EXISTING NORTH EAST ELEVATION
1:100



EXISTING SOUTH EAST ELEVATION
1:100



PROPOSED NORTH EAST ELEVATION
1:100




PROPOSED SOUTH EAST ELEVATION
1:100

RevID	CHD	Revision	DATE

CLIENT
PAIHIA TERRACES Ltd.

ADDRESS
**30-34 SELWYN ROAD,
PAIHIA
BAY OF ISLANDS
FAR NORTH DISTRICT COUNCIL**

PROJECT
DEVELOPMENT STAGE 1



CTM
C.T.M. ARCHITECTURAL LTD.

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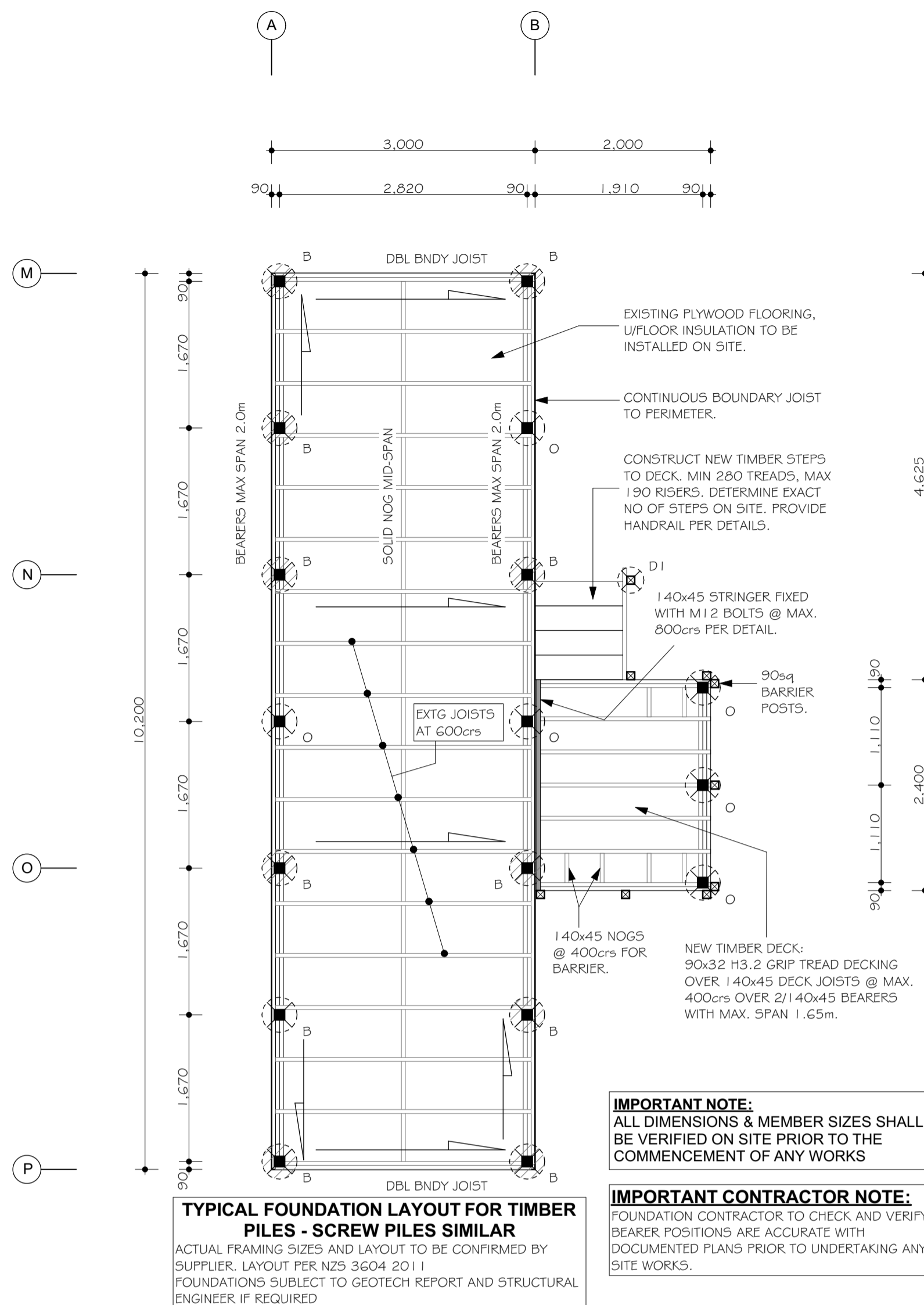
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SCHEME PRELIM RC

DRAWING NAME
ELEVATIONS

DRAWN CRAIG	SCALE AS SHOWN	A08
DATE 18 DEC 2024	SHEET FORMAT A1	
JOB REF 24-059	TOTAL SHEETS 13	SHEET REVISION



TIMBER SUB-FLOOR KEY:

ALL DEPTHS ARE BELOW CLEARED GROUND LEVEL (C.G.L.) AND THIS SHALL BE ALLOWED FOR IF THE PLATFORM IS NOT CLEARED OF TOPSOIL ETC PRIOR TO FOUNDATION WORKS.

MIN STRENGTH FOR CONC FOOTINGS = 17.5MPa

PILES & CONC FOOTING SIZES AS PER BELOW:

○ DENOTES ORDINARY SQUARE PILES
USE 125x125sq TIMBER PILE EMBEDDED 500mm (FOR MIN 100mm BASE COVER) INTO MIN. 400Ø x 600mm DEEP CONC. PILE FOOTING, SET MIN. 600 BELOW C.G.L. ORDINARY PILE FIXINGS AT TOP.

⊠ DENOTES BRACED SQUARE PILES
USE 125x125sq TIMBER PILE EMBEDDED 500mm (FOR MIN 100mm BASE COVER) INTO MIN. 400Ø x 600mm DEEP CONC. PILE FOOTING, SET MIN. 600 BELOW C.G.L. 12ØN PILE FIXINGS AT TOP.

⊠ DENOTES ORDINARY DECK BARRIER POST IN FOOTING
USE 90x90sq TIMBER POSTS EMBEDDED 500mm (FOR MIN 100mm BASE COVER) INTO MIN. 400Ø x 600mm DEEP CONC. PILE FOOTING. ORDINARY PILE FIXINGS AT TOP.

SUBSTITUTE ANCHOR PILES WITH BRACED PILE SYSTEM (IN BOTH DIRECTIONS) WHERE PILE HEIGHT EXCEEDS MAX 600mm.

⊠ DENOTES SUBFLOOR BRACE, SIZE AS FOLLOWS
(ARROW POINTS TO LOWER END)
NOT EXCEEDING 3.0m - 100x75
NOT EXCEEDING 5.0m - 100x100

PILE NOTE:

PILES TO BE MINIMUM 150mm HIGH ABOVE FINISHED GROUND LEVEL. INSTALL DPC BENEATH BEARERS WHERE PILES ARE LESS THAN 300mm HIGH.

SUBFLOOR VENTING

SUBFLOOR VENTILATION TO BE PROVIDED BY TIMBER BASE BOARDS WITH MINIMUM 20mm GAPS GIVING REQUIRED VENTILATION AS PER NZS 3604.

SUBFLOOR NOTES:

GROUND LEVEL VARIES. PROVIDE MIN 450mm CRAWL SPACE TO USIDE OF HOUSE JOISTS.

INSTALL BASE BOARDS WITH MIN 20mm VENTING GAPS TO SUB FLOOR FRAME.

PROVIDE SUBFLOOR ACCESS HATCH.

HOUSE BEARERS MAX. SPAN 2.0m. ENSURE SOLID BLOCKING AT 1800c/s MAX OVER ALL BEARER SUPPORTS.

C.O.S POSITION OF ANY BEARER JOINS & ENSURE A PILE IS LOCATED DIRECTLY BENEATH. ADJUST PILES TO SUIT IF REQUIRED (MAX. BEARER SPAN AS NOTED).

ENSURE MIN. 12mm GAP BETWEEN NEW DECKS & CLADDING

IMPORTANT NOTE:
ALL DIMENSIONS & MEMBER SIZES SHALL BE VERIFIED ON SITE PRIOR TO THE COMMENCEMENT OF ANY WORKS

IMPORTANT CONTRACTOR NOTE:
FOUNDATION CONTRACTOR TO CHECK AND VERIFY BEARER POSITIONS ARE ACCURATE WITH DOCUMENTED PLANS PRIOR TO UNDERTAKING ANY SITE WORKS.

TYPICAL FOUNDATION LAYOUT FOR TIMBER PILES - SCREW PILES SIMILAR

ACTUAL FRAMING SIZES AND LAYOUT TO BE CONFIRMED BY SUPPLIER. LAYOUT PER NZS 3604:2011 FOUNDATIONS SUBJECT TO GEOTECH REPORT AND STRUCTURAL ENGINEER IF REQUIRED

TYPICAL FOUNDATION PLAN - UNIT 1
1:50

GIB EzyBrace® Bracing Software

Demand Calculation Sheet

Job Name: Proposed Relocation of Dwelling

Name: 30-34 Selwyn Street, Pahiia
Lot and DP Number: Lot 1, 2 & 3 DP 328135
City/Town/District: Far North Council
Designer: Sheryl
Company: CTM Architectural Ltd
Date: Friday, 1 November 2024

Building Specification

Number of Storeys: 1
Floor Loading: 2 kPa
Foundation Type: Subfloor
Subfloor Cladding Weight: Light

Cladding Weight
Single
Cladding Weight: Light
Roof Weight: Light
Room in Roof Space: No
Roof Pitch (degrees): 3
Roof Height above Eaves (m): 0.3
Building Height to Apex (m): 3.5
Ground to Lower Floor (m): 1.0

Average Stud Height (m): 2.4
Building Length (m): 10.2
Building Width (m): 3.0
Building Plan Area (m²): 30.60

Building Location

Wind Zone = High
Earthquake Zone 1
Soil Type: D & E (Deep to Very Soft)
Annual Prob. of Exceedance: 1 in 500 (NZS3604:2011 Default)

Bracing Units required for Wind		Bracing Units required for Earthquake	
	Along	Across	Along & Across
Single Level	84	255	140
Subfloor Level	386	614	205

GIB EzyBrace® Bracing Software

Subfloor Level Along Resistance Sheet

Job Name: Proposed Relocation of Dwelling

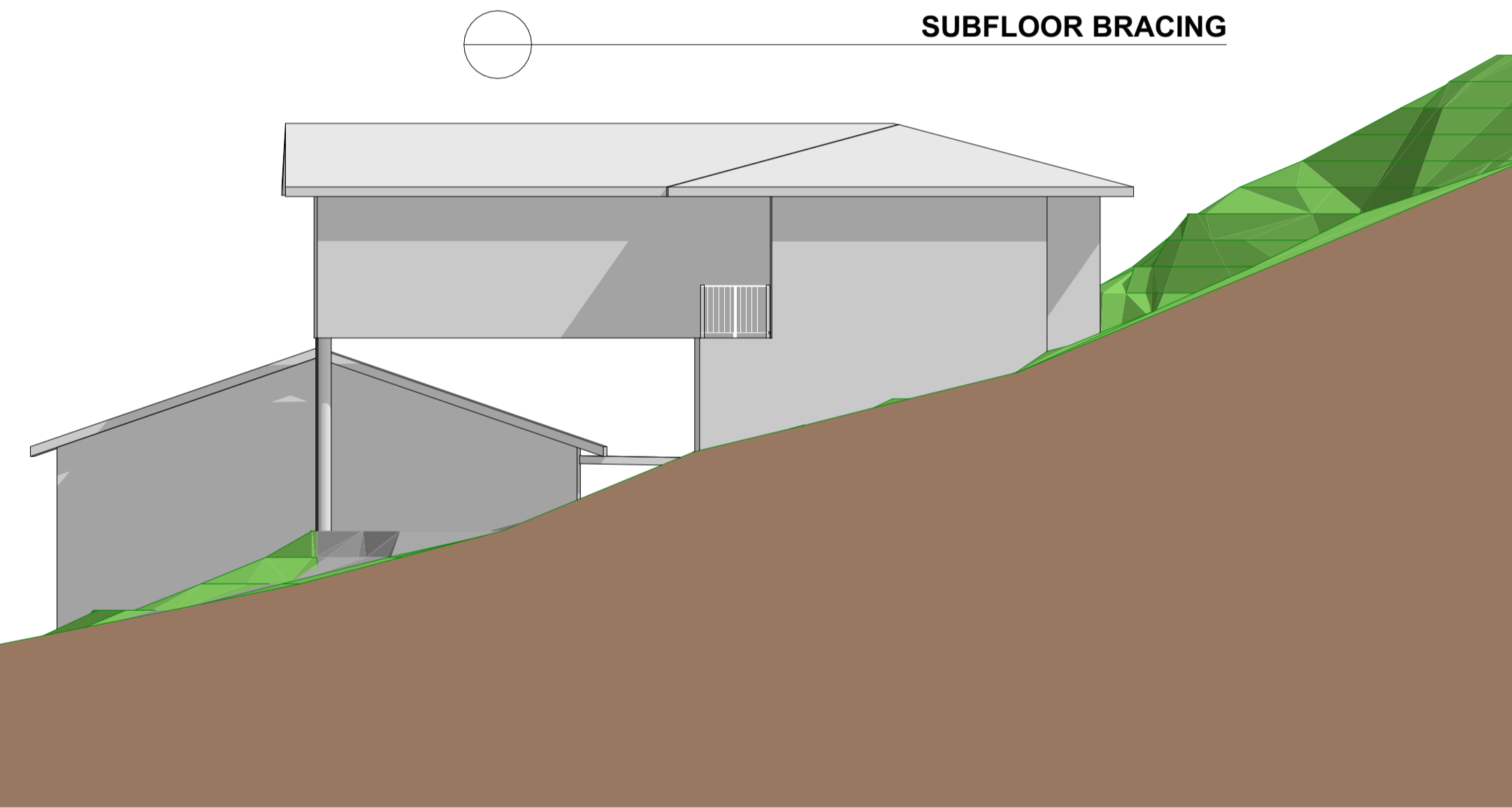
Line	Element	Length (m)	Angle (degrees)	Type	Supplier	Wind (kPa)	EQ (kPa)	Demand		Achieved	
								180	205	180	205
A	1	2.00		Braced Pile	NZS3604	320	240	124%	178%	180 OK	205 OK
B	1	1.00		Braced Pile	NZS3604	180	120	100 OK	120 OK	180 OK	120 OK

GIB EzyBrace® Bracing Software

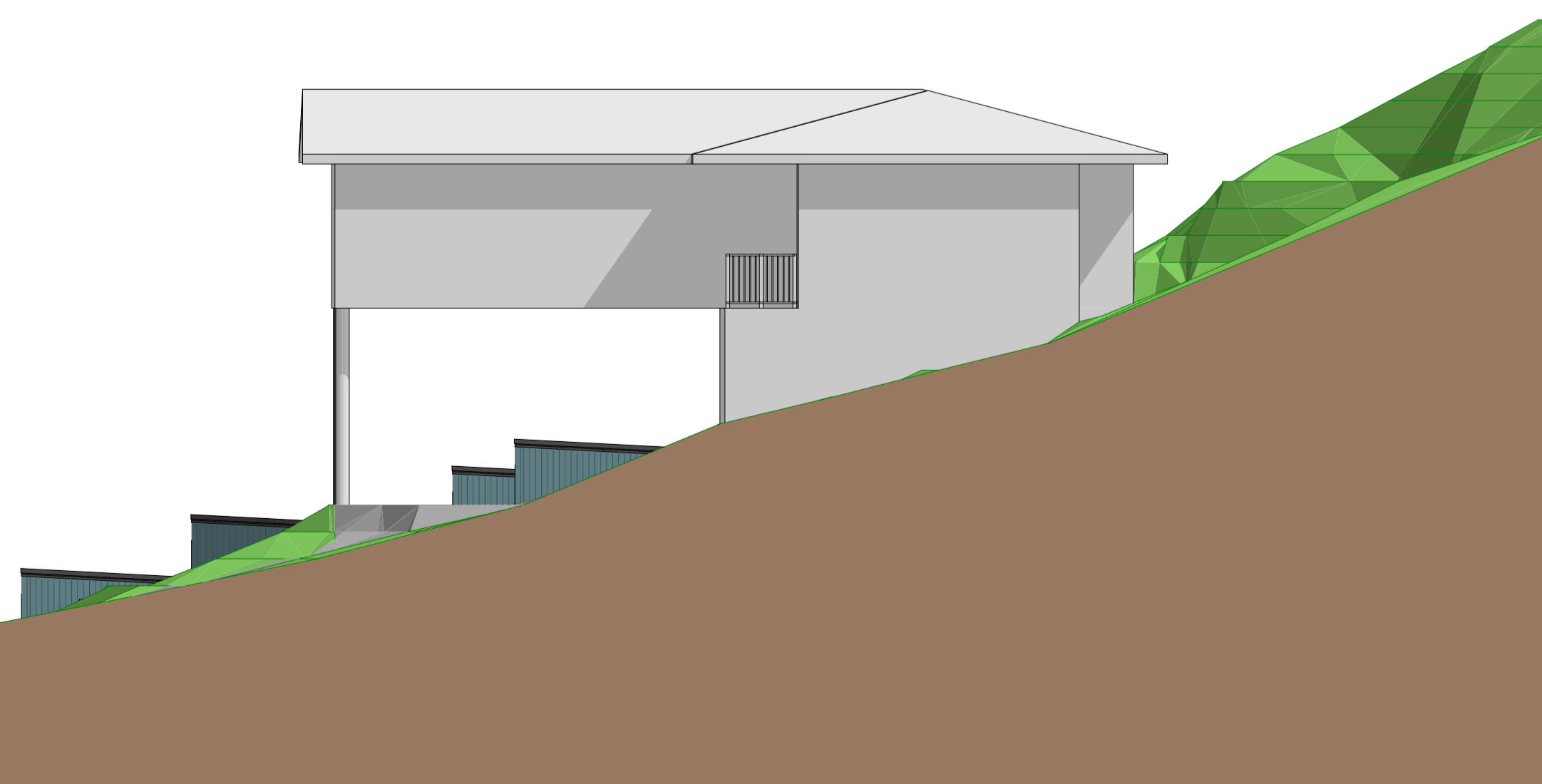
Subfloor Level Across Resistance Sheet

Job Name: Proposed Relocation of Dwelling

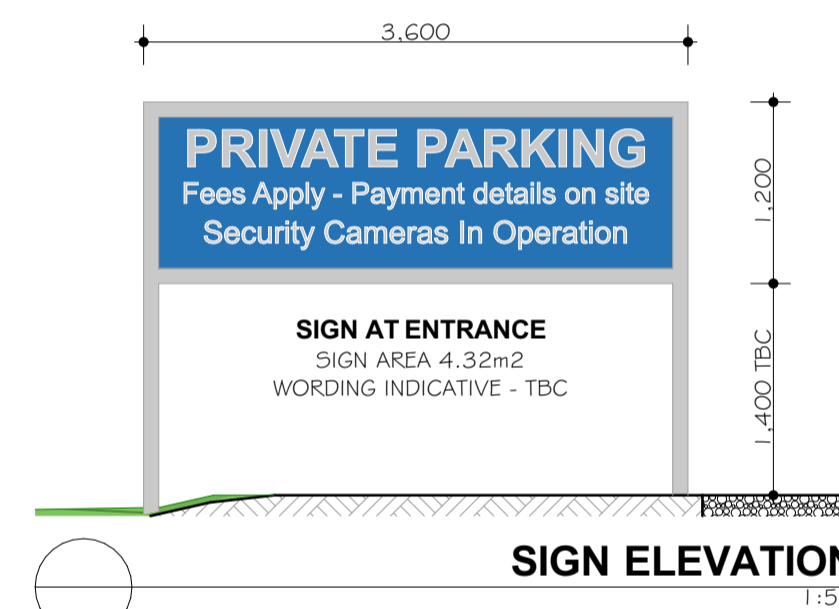
Line	Element	Length (m)	Angle (degrees)	Type	Supplier	Wind (kPa)	EQ (kPa)	Demand		Achieved	
								180	205	180	205
M	1	1.00		Braced Pile	NZS3604	180	120	100 OK	120 OK	180 OK	120 OK
N	1	1.00		Braced Pile	NZS3604	180	120	100 OK	120 OK	180 OK	120 OK
O	1	1.00		Braced Pile	NZS3604	180	120	100 OK	120 OK	180 OK	120 OK
P	1	1.00		Braced Pile	NZS3604	180	120	100 OK	120 OK	180 OK	120 OK



EXISTING NORTH WEST ELEVATION
1:100



PROPOSED NORTH WEST ELEVATION
1:100



SIGN ELEVATION
1:50

RevID	CHD	Revision	Date

CLIENT
PAIHIA TERRACES Ltd.

ADDRESS
**30-34 SELWYN ROAD,
PAIHIA
BAY OF ISLANDS
FAR NORTH DISTRICT COUNCIL**

PROJECT
DEVELOPMENT STAGE 1

CTM
C.T.M. ARCHITECTURAL Ltd.

CTM ARCHITECTURAL LTD
18 SWEETCORN PLACE,
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e-mail: info@ctm.net.nz

WORK TO NZ B.C. AND RELEVANT CODES
DIMENSIONS SUPERSEDE SCALE
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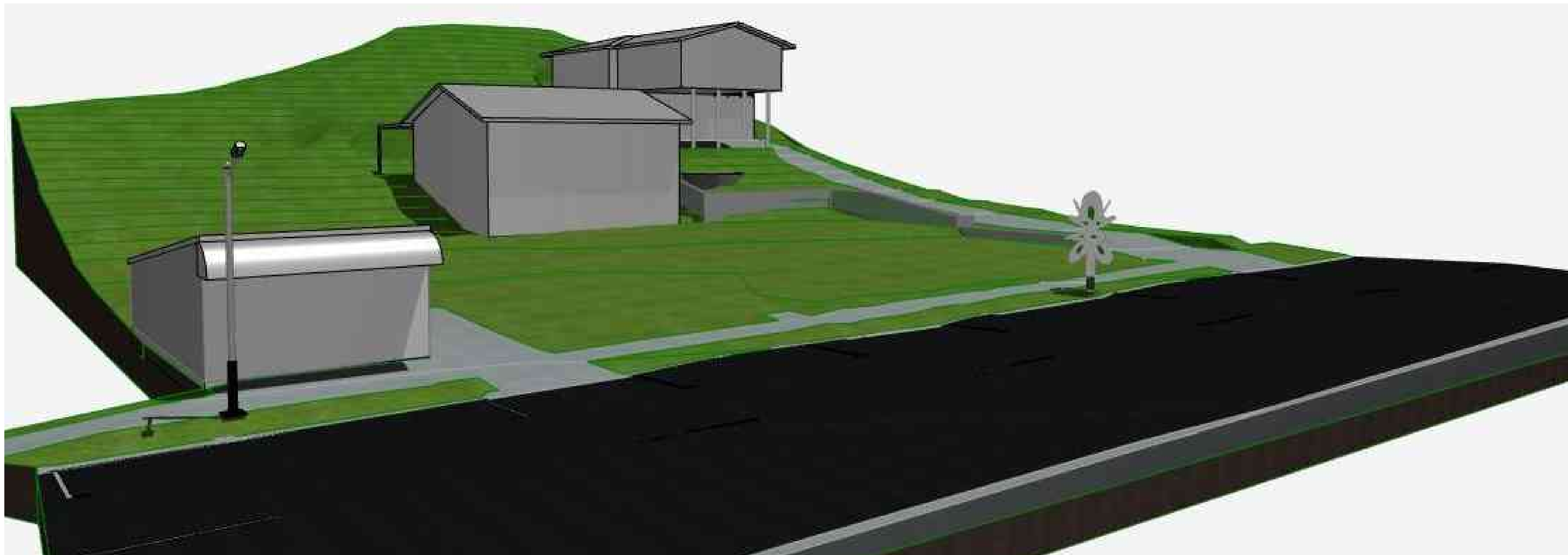
ISSUE
SCHEME PRELIM RC

DRAWING NAME
ELEVATIONS

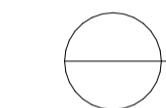
DRAWN	SCALE	SHEET NO.	TOTAL SHEETS	SHEET	REVISION
CRAIG	AS SHOWN				

DATE
18 DEC 2024

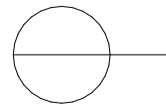
JOB REF
24-059



EXISTING



PROPOSED



RevID	CHD	Revision	DATE
CLIENT			
PAIHIA TERRACES Ltd.			
ADDRESS			
30-34 SELWYN ROAD, PAIHIA BAY OF ISLANDS FAR NORTH DISTRICT COUNCIL			
PROJECT			
DEVELOPMENT STAGE 1			



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ISSUE

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DRAWING NAME

VISUALS 1

DRAWN	SCALE	A10
CRAIG	AS SHOWN	
DATE	SHT FORMAT	
18 DEC 2024	A1	
JOB REF	TOTAL SHTS	
24-059	13	

SHEET REVISION



EXISTING



PROPOSED

RevID	CHD	Revision	Date

CLIENT
PAIHIA TERRACES Ltd.
 ADDRESS
**30-34 SELWYN ROAD,
 PAIHIA
 BAY OF ISLANDS
 FAR NORTH DISTRICT COUNCIL**
 PROJECT
DEVELOPMENT STAGE 1



CTM ARCHITECTURAL LTD
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 PUNAKOHE, 2040
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ISSUE
SCHEME PRELIM RC
 DRAWING NAME
VISUALS 2

DRAWN CRAIG	SCALE AS SHOWN	A11	REVISION
DATE 18 DEC 2024	SHEET FORMAT A1		
JOB REF 24-059	TOTAL SHEETS 13		
SHEET			



PROPOSED



PROPOSED



PROPOSED



PROPOSED



PROPOSED

RevID	CHD	Revision	Date

CLIENT
PAIHIA TERRACES Ltd.

ADDRESS
**30-34 SELWYN ROAD,
PAIHIA
BAY OF ISLANDS
FAR NORTH DISTRICT COUNCIL**

PROJECT
DEVELOPMENT STAGE 1



CTM ARCHITECTURAL LTD
18 SWEETCORN PLACE,
PUNGIKONE, 2100
PO Box 547
PUNGIKONE, 2040
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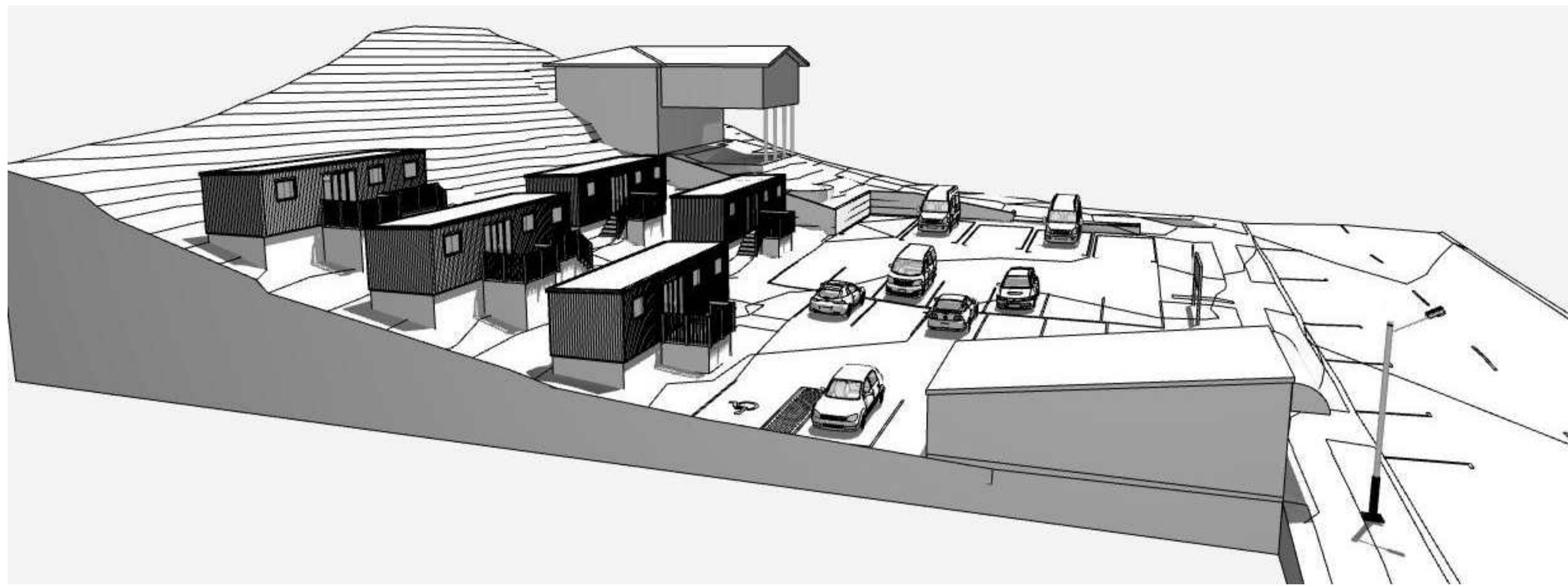
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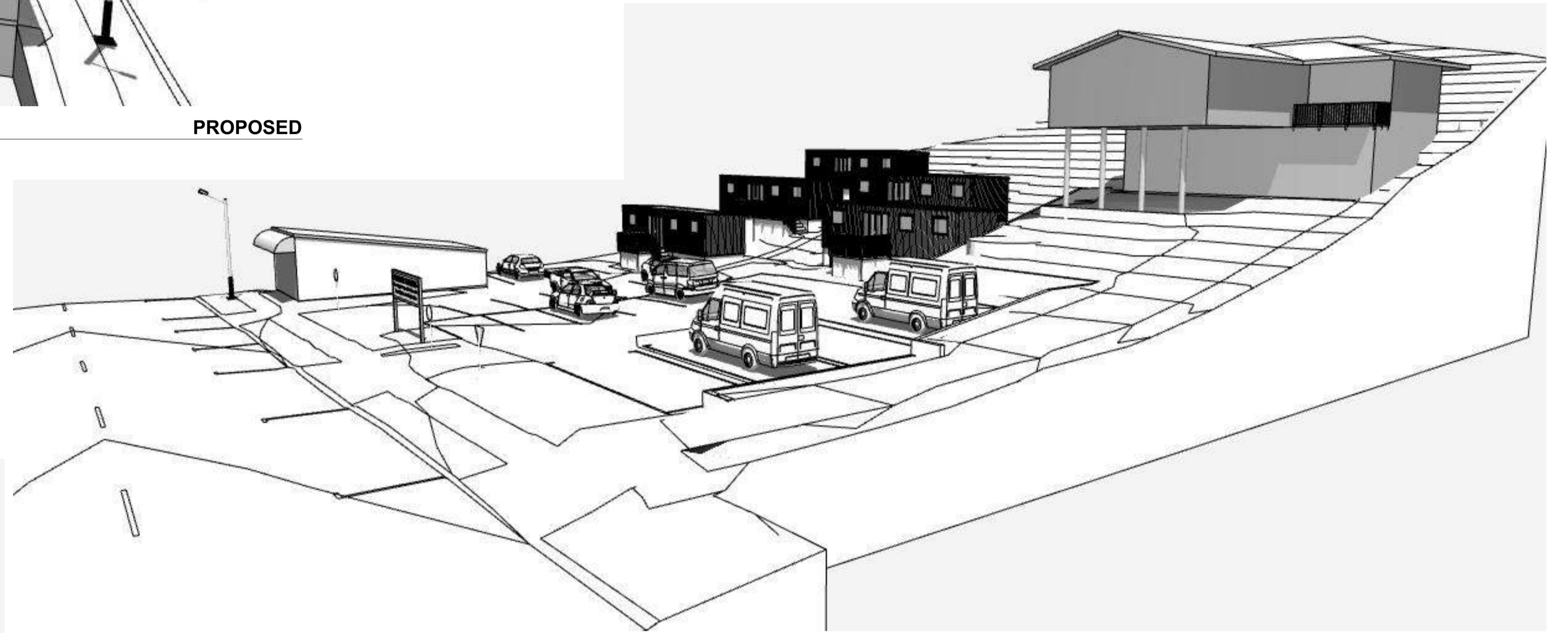
ISSUE
SCHEME PRELIM RC

DRAWING NAME
VISUALS 3

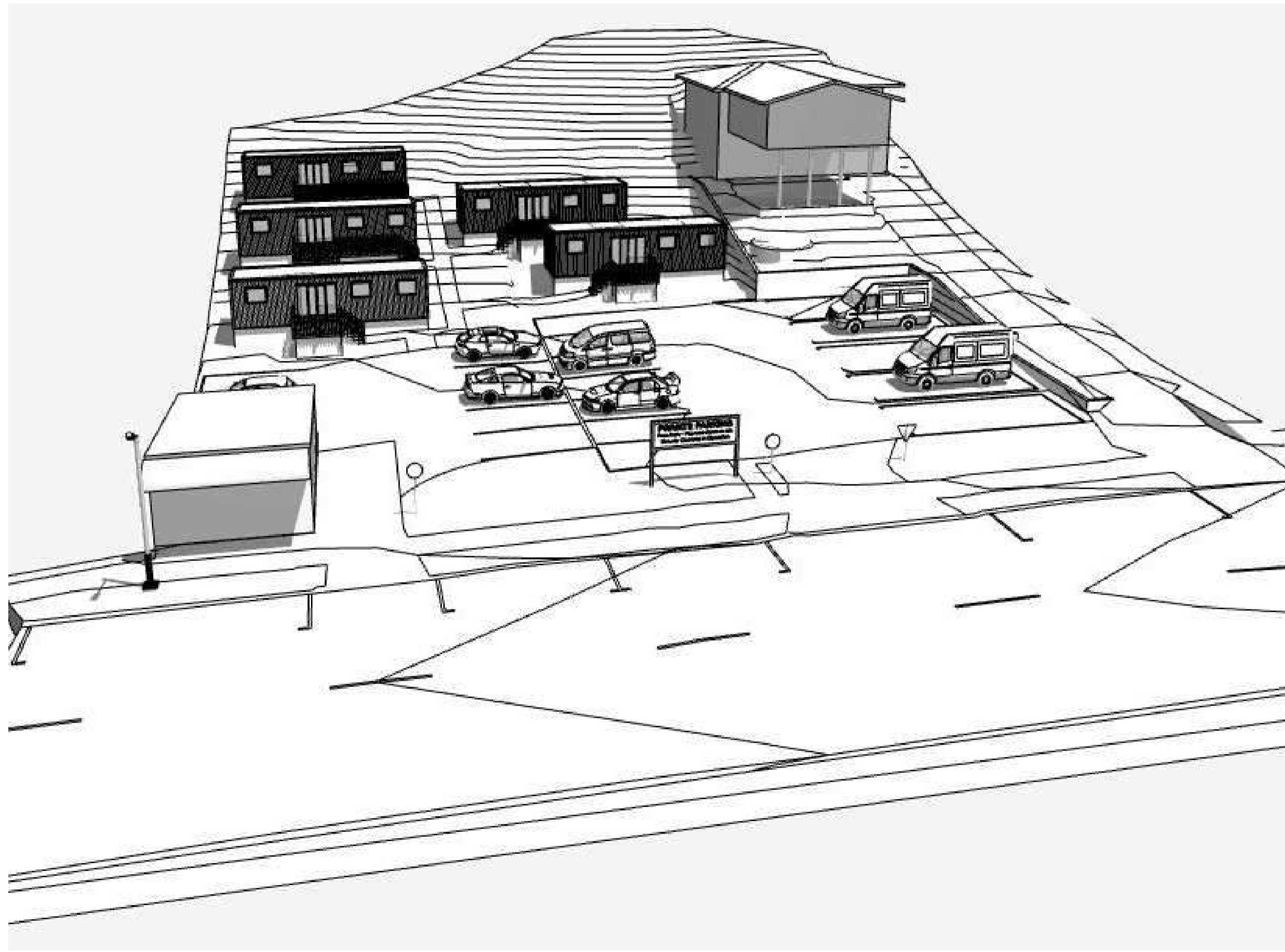
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DATE 18 DEC 2024	SHEET FORMAT A1			
JOB REF 24-059	TOTAL SHEETS 13			



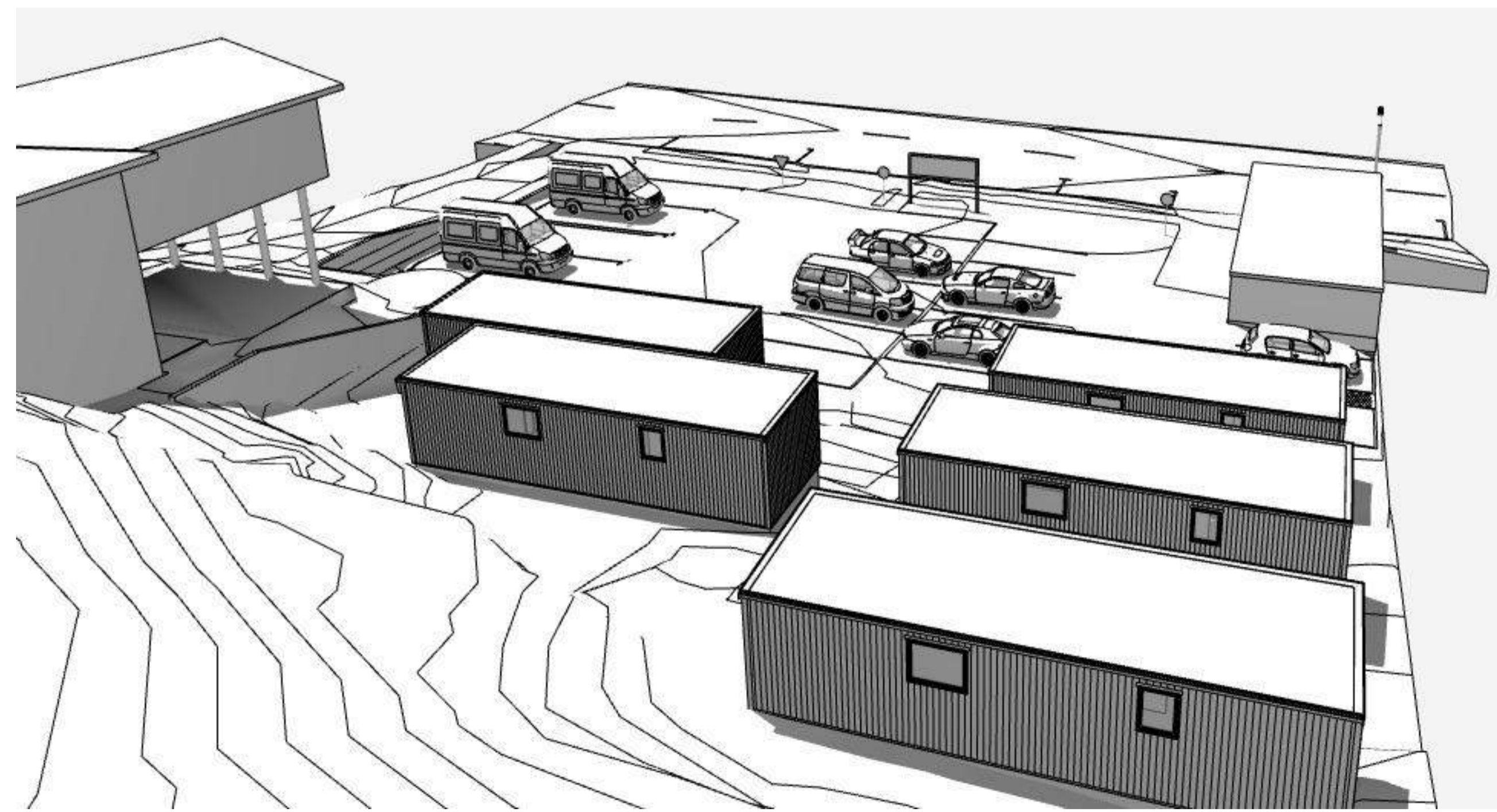
PROPOSED



PROPOSED



PROPOSED



PROPOSED

RevID	CHD	Revision	DATE
CLIENT PAIHIA TERRACES Ltd.			
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PROJECT DEVELOPMENT STAGE 1			



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ISSUE
SCHEME PRELIM RC

DRAWING NAME
WHITE MODEL

DRAWN CRAIG	SCALE AS SHOWN	A13
DATE 18 DEC 2024	SHEET FORMAT A1	
JOB REF 24-059	TOTAL SHEETS 13	SHEET REVISION

Project Proposal

Kyron Wealth Mentors
Version Number 1.0



Presented To
Kyron Gosse

Presented by
Ida-Jean Murray

Kia Ora Kyron,

We at North Build are excited to partner with you on this meaningful journey to create spaces that resonate with your mission of helping everyday heroes find purpose in their work. We understand the importance of providing quality and affordable living spaces, and our aim is to ensure that these cabins not only meet your needs but also foster a sense of community and belonging.

About North Build

North Build exists solely to provide an employment pathway for our rangatahi. We introduce them to work life in a safe, progressive, and whanau-inclusive environment that helps them thrive and reach their full potential. Our secondary goal is to provide affordable housing solutions for our team, whanau, hapū, and iwi

North Build Ltd

- Cabins
- Transportable Homes
- Renovations
- Decks & Fencing
- Chillers

North Drill Ltd

- Site Works
- Drainage
- Utilities- Power, Water, Sewer

Whatever you want, the way you want it; always at a competitive price.



Proposal Overview

We're offering our 10.2m x 3.0m self-contained cabin, perfect for compact and affordable living. It's designed to be a comfortable and practical space where you can unwind and feel at ease.



Cabin Details

Size & Build:

- **Size:** 30m², offering plenty of space and comfort.
- **Insulation:** Double glazing and 75mm insulated panels for optimal warmth.

Interior:

- **Bedroom:** Two double bedrooms, each designed for practical living
- **Kitchenette:** Features space for an under-bench fridge, 2-burner gas hob, range hood, and ample storage.
- **Bathroom:** Equipped with a shower, toilet, vanity, and macerator plumbing.
- **Living Area:** A open area perfect for relaxing and entertaining.
- **Floors:** Stylish hardwood click-lock flooring throughout.
- **Windows:** Five double-glazed windows that bring in plenty of natural light.



Utilities

- **Power & Lights:** 8 double points, and lighting.
- **Hot Water:** Gas Califont
- **Electrical & Gas certified**

Design & Floor Plan

We've designed this 10.2m x 3.0m self-contained cabin with your comfort in mind. On this page, you'll see the layout and features that make it a welcoming and cozy space for you and your whanau



Pricing & Timings

We've priced each cabin based on the features above. We will work with you to get these built and delivered when you need them.

Description	Amount
Cabin: 10.2m x 3.0m Self-contained Transport: Whangarei to Paihia <ul style="list-style-type: none"> • Price incudes transport to level site with easy access for Hiab 	
TOTAL	\$85,000.00 (Including GST)

Terms & Conditions

Payment Terms: A 50% deposit is required before work commences. The remaining balance is due three days before delivery.

Project Timeline: The estimated timeline for project completion will be provided upon acceptance of the proposal. Any delays caused by unforeseen circumstances will be communicated promptly.

Variations: Any changes to the design or project specifications after quote acceptance may incur additional costs and adjustments to the timeline. All changes must be agreed upon in writing.

Warranty: All work and materials are covered by a standard manufacturers warranty.

Site Access: The proposed price is based on the cabin being transported to a level site with easy access. Any additional earthworks required will incur extra charges

Liability: North Build Ltd is not liable for any delays, damage, or additional costs caused by factors outside of our control, including but not limited to weather conditions, supply chain disruptions, or third-party actions.

Acceptance of Quote: Acceptance of this proposal constitutes an agreement to the terms and conditions outlined herein. The project will commence once the proposal is accepted and any required deposit is received.

Thank you for the opportunity to provide this proposal. We look forward to the possibility of working together

For inquiries, contact us.



Ida-Jean Murray | Managing Director
0272 999 134
ida@northdrill.co.nz

Bronson Murray | Managing Director
022 430 6543
Bronson@northdrill.co.nz





BERNARD CALLINAN
PERMACULTURE DESIGN



Note:

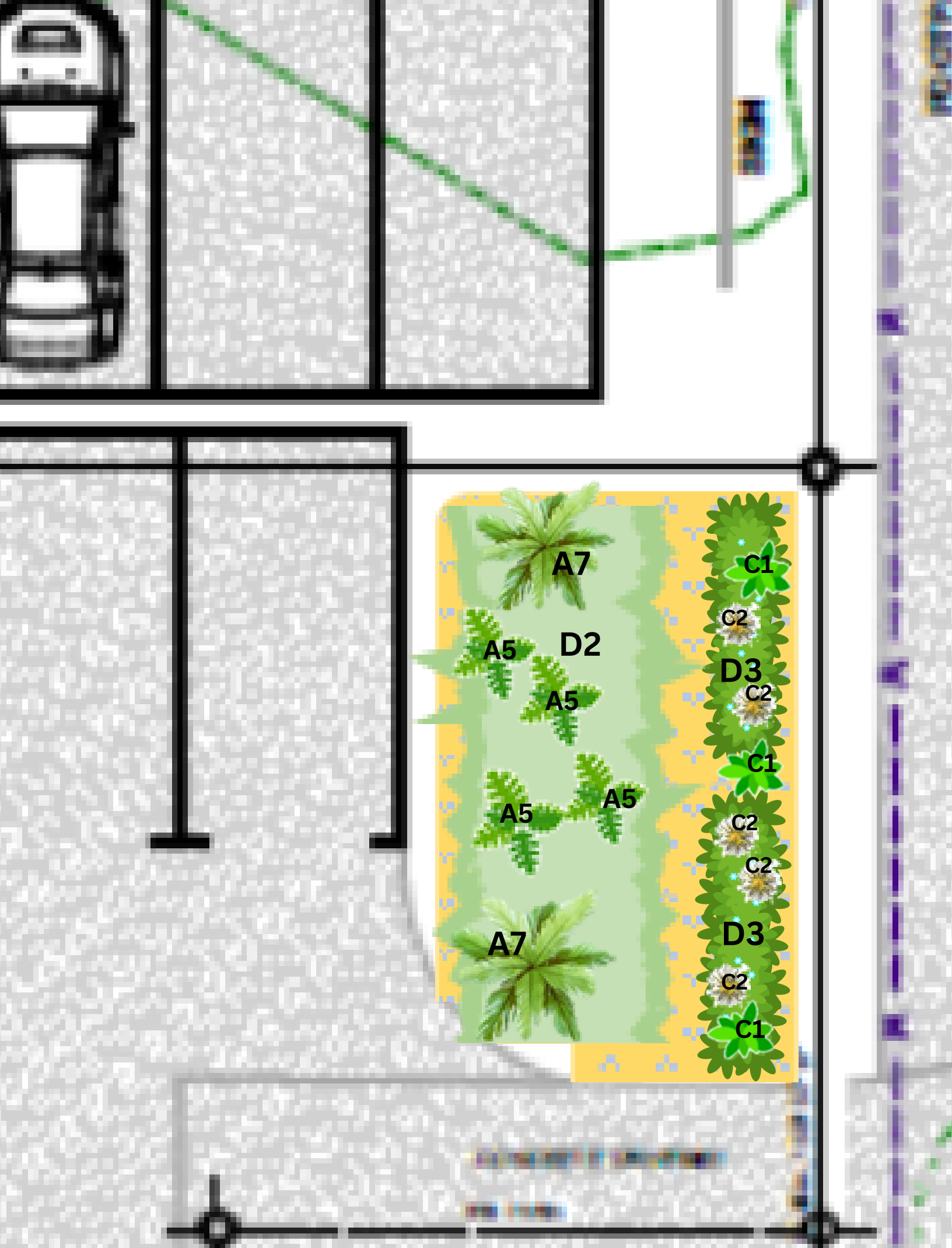
Plant symbols, placings and information remain the property of Bernard Callinan 'Permaculture Design'.

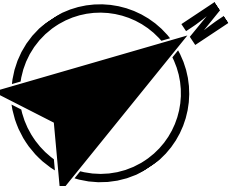
Background schematics are courtesy of CTM Architectural Ltd and remain their property.

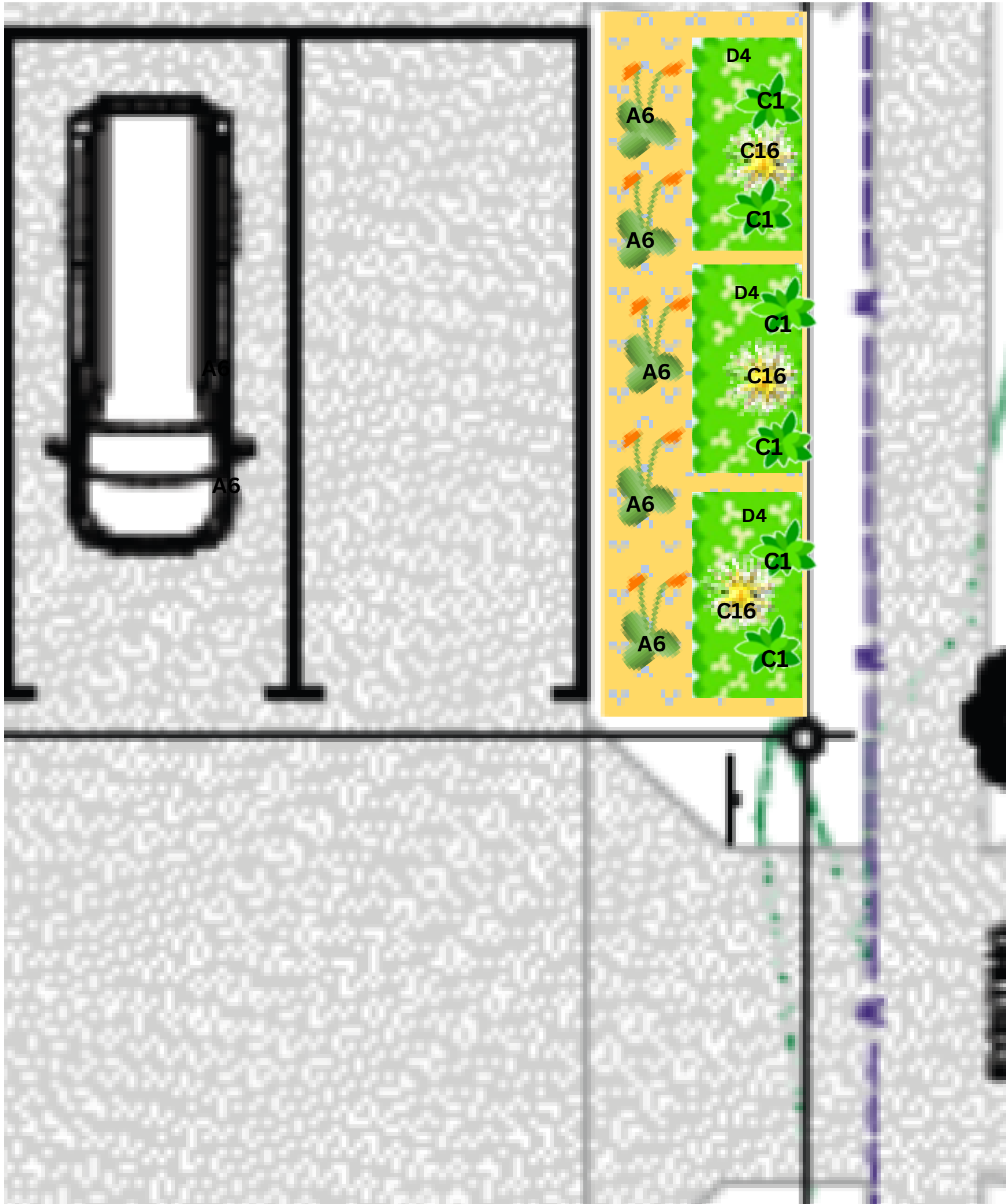
Sizing of plants is indicative. Full information on plants, purposes, sizes and attractions are covered in an accompanying Excel



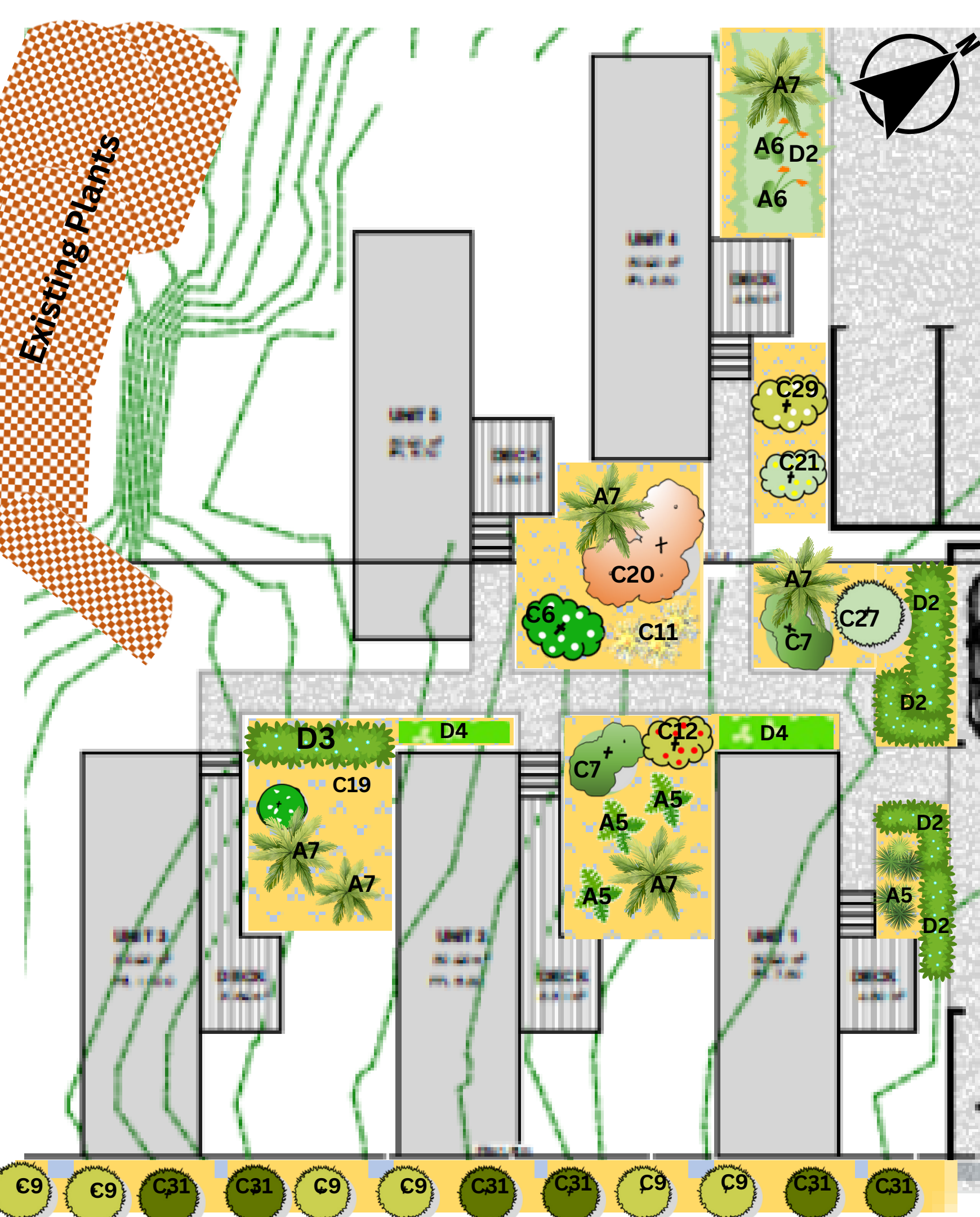
Planting along the street is predominately groundcovers along with placements of palm, succulents and grasses with heights ranging from ½ mtr to 2 mtrs (palms).
If a low mulch bed is laid out for plants to be planted in, thus slightly raising the plantings, the view from the street can be softened even more.



Front Left		
A7	<i>Syagrus romanzoffiana</i>	Queen Palm, Cocos Palm
A5	<i>Sabal minor</i>	Dwalf Palmetto
C1	<i>Agave attenuata</i>	Agave
C2	<i>Anemanthele lessoniana</i>	Gossamer Grass, New Zealand Wind Grass, Pheasant's Tail Grass, Hunangamoho
D2	<i>Coprosma</i>	Coprosma
D3	<i>Grevillea</i>	



Front		
Right		
A6	<i>Strelitzia reginae</i>	Bird of Paradise Flower, Crane Flower, Strelitzia
C1	<i>Agave attenuata</i>	Agave
C16	<i>Lomandra longifolia</i> c.'Lime Tuff'	Mat Rush
D4	<i>Pratia</i>	



A5	Sabal minor	Dwalf Palmetto
A6	Strelitzia reginae	Bird of Paradise Flower
A7	Syagrus romanzoffiana	Queen Palm, Cocos Palm
C2	Anemanthele lessoniana	Gossamer Grass
C3	Apodasmia similis	OiOi
C6	Carissa macrocarpa	Natal Plum
C7	Cassia fistula	Indian Laburmun
C9	Cytisus proliferus	Tagasaste, Tree Lucern
C11	Dierama pulcherrimum	Angel's Fishing Rod
C12	Grevillea dimorpha	Flame Grevillea
C18	Plumeria obtusa	White Frangipani
C19	Plumeria rubra	Red Frangipani
C20	Senna (Cassia) alata	Candle Bush
C21	Senna (Cassia) artemisioides	Silver Cassia, Punty
C27	Tibouchina lepidota	Alstonville, [Lasiandra)
C29	Trachelospermum jasminoides	Star Jasmine,
D2	Coprosma	Coprosma
D3	Grevillea	Grevillea Groundcovers
D4	Pratia	Pratia Groundcovers



HODGSON
PLANNING CONSULTANTS

Far North District Plan Assessment

Paihia Terraces Ltd
30 to 34 Selwyn Road,
Paihia

HPC Reference PT2419

December 2024

District Plan Assessment

Operative Far North District Plan 2009

Applicant: Paihia Terraces Ltd

Site Address: 30, 32, 34 Selwyn Road, Paihia

Legal Description: Lot 1 DP 328135, RT NA114614. 829m²
Lot 2 DP 328135, RT NA114615, 691m²
Lot 3 DP 328135, NA114616) 760m²

Operative Far North District Plan 2009

Zone: Paihia Commercial
Sub-Zone: Area A3
Zone Overlay: Pedestrian Frontage

Proposed Far North District Plan, Notified 2022

Zone: Mixed Use
Sub-Zone: Area B
Zone Overlay: Pedestrian Frontage
Environment: Coastal

Proposal: Land Use Consent for a maximum duration of five years to establish the following:

- five relocatable cabins to provide for visitor accommodation;
- seven commercial car parking spaces (daily tariff);
- Overnight parking for up to five self-contained campervans;
- Associated signage, landscaping, earthworks and infrastructure required to establish the above activities within these sites.

Overall Activity Status: Restricted Discretionary

Operative Far North District Plan 2009

Part 2 – Environment Provisions		
Chapter 7 - Urban Environment		
7.3	Objectives	Comments
7.3.1	<i>To ensure that urban activities do not cause adverse environmental effects on the natural and physical resources of the District.</i>	The proposal includes appropriate mitigation measures to avoid adverse environmental effects on the natural and physical resources of the district.
7.3.2	<i>To enable the continuing use of buildings and infrastructure in urban areas, particularly where these are under-utilised.</i>	The existing shop at 30 Selwyn Road and the existing dwelling at 34 Selwyn Road will remain in use. The dwelling at 32 Selwyn Road has been demolished. The existing vehicle crossings will be retained and upgraded.
7.3.3	<i>To avoid, remedy or mitigate the adverse effects of activities on the amenity values of existing urban environments.</i>	A temporary consent for a duration of up to five years is sought for the proposed visitor accommodation and visitor parking activities.
7.3.4	<i>To enable urban activities to establish in areas where their potential effects will not adversely affect the character and amenity of those areas.</i>	These are activities consisted with land uses located within adjacent sites within the Commercial zone.
7.3.5	<i>To achieve the development of community services as an integral and complementary component of urban development.</i>	Not applicable.
7.3.6	<i>To ensure that sufficient water storage is available to meet the needs of the community all year round.</i>	The site is located within an urban environment and following a Pre-Application Meeting, the Resource Consents Engineer has advised that the cabins can be served by the town water supply system, and booster pumps may be required for those located cabins located on the hillslope.
7.4	Policies	
7.4.1	<i>That amenity values of existing and newly developed areas be maintained or enhanced.</i>	The application seeks consent to establish activities for a limited duration of up to five years. Appropriate mitigation measures are proposed as part of the application including landscaping to 'soften' the extent of impervious surface areas and contribute to maintaining the amenity of the area.
7.4.2	<i>That the permissible level of effects created or received in residential areas reflects those appropriate for residential activities.</i>	Not applicable. The site is zoned Commercial.
7.4.3	<i>That adverse effects on publicly-provided facilities and services be avoided or remedied by new development, through the provision of additional services.</i>	The proposed activities can be serviced by appropriate infrastructure and includes the on-site management of stormwater (refer Stormwater Management Report in Attachment 4).

7.4.4	<i>That stormwater systems for urban development be designed to minimise adverse effects on the environment.</i>	The proposal includes an on-site stormwater management system to avoid any adverse effects including downstream flood hazards.
7.4.5	<p><i>That new urban development avoid:</i></p> <p><i>(a) adversely affecting the natural character of the coastal environment, lakes, rivers, wetlands or their margins;</i></p> <p><i>(b) adversely affecting areas of significant indigenous vegetation or significant habitats of indigenous fauna;</i></p> <p><i>(c) adversely affecting outstanding natural features, landscapes and heritage resources;</i></p> <p><i>(d) adversely affecting the relationship of Māori and their culture and traditions with their ancestral lands, water, sites, waahi tapu, and other taonga;</i></p> <p><i>(e) areas where natural hazards could adversely affect the physical resources of urban development or pose risk to people’s health and safety;</i></p> <p><i>(f) areas containing finite resources which can reasonably be expected to be valuable for future generations, where urban development would adversely affect their availability;</i></p> <p><i>(g) adversely affecting the safety and efficiency of the roading network;</i></p> <p><i>(h) the loss or permanent removal of highly productive and versatile soils from primary production due to subdivision and development for urban purposes.</i></p>	<p>The proposal will not adversely affect the natural character of the coastal environment, lakes, rivers, wetlands or their margins. A low rise development is proposed for a limited duration (5 years).</p> <p>The site does not contain areas of significant indigenous vegetation or significant habitats of indigenous fauna.</p> <p>The proposal seeks to undertake commercial and residential activities within an existing urban environment where there are no known cultural heritage sites.</p> <p>The proposal includes an on-site stormwater management system to avoid adverse effects associated with stormwater runoff/discharges upon the environment including adverse downstream flooding.</p> <p>A Traffic Impact Assessment has been provided that confirms that the proposed activities will not adversely affect the safety and efficiency of the roading network.</p> <p>The site is within an existing urban environment and does not include the removal of highly productive and versatile soils from primary production.</p>
7.4.6	<i>That the natural and historic heritage of urban settlements in the District be protected (refer to Chapter 12).</i>	The site does not contain any sites of natural or historic heritage.
7.4.7	<i>That urban areas with distinctive characteristics be managed to maintain and enhance the level of amenity derived from those characteristics.</i>	<p><i>“Paihia is one of the most tourism-orientated settlements in the District... Geographically, Paihia has a flattish central area surrounded by steep to rolling hills.¹”</i></p> <p>The proposed activities seek to provide visitor accommodation and parking, providing services to accommodate visitors to Paihia.</p> <p>Flatter areas within the site are located adjacent to Selwyn Road, then the land rises</p>

¹ Far North District Plan, Operative 2009, refer Chapter 1 – Introduction, 1.4.3.2 *Description of the Urban Environment, Paihia*, page 8.

		<p>steeply to the southwest. The development is proposed within the flatter/more moderate slopes within the site to minimise earthworks and changes to the landform.</p> <p>The proposal seeks consent to establish and operate temporary activities (maximum duration of five years) and includes the provision of appropriate infrastructure and landscaping to maintain amenity values.</p>
7.4.8	<p><i>That infrastructure for urban areas be designed and operated in a way which:</i></p> <p><i>(a) avoids, remedies or mitigates adverse effects on the environment;</i></p> <p><i>(b) provides adequately for the reasonably foreseeable needs of future generations; and</i></p> <p><i>(c) safeguards the life-supporting capacity of air, water, soil and ecosystems.</i></p>	<p>The proposal includes provision of appropriate infrastructure to service the proposed activities to avoid, remedy or mitigate adverse effects on the environment; provides for the reasonably foreseeable needs of future generations; and safeguards the life-supporting capacity of air, water, soil and ecosystems.</p>
7.4.9	<p><i>That the need for community services in urban areas is recognised and provided for.</i></p>	<p>Not applicable.</p>
7.7 - Commercial Zone		
7.7.3	Objectives	
	<i>These objectives supplement those set out in Section 7.3.</i>	
7.7.3.1	<p><i>To achieve the development of commercial areas in the District accommodating a wide range of activities that avoid, remedy or mitigate the adverse effects of activities on other activities within the Commercial Zone and on the natural and physical resources of the District.</i></p>	<p>The proposed activities are consistent with those anticipated to occur within the Commercial zone and appropriate measures are proposed to avoid, remedy or mitigate adverse effects on other activities within the zone and on the natural and physical resources of the district.</p> <p>The proposal includes the use of relocatable cabins to provide for visitor accommodation and metalled areas for the overnight parking of self-contained campervans and cars. Measures to avoid adverse effects include setbacks from boundaries, consent conditions relating to noise and the provision of appropriate infrastructure to service the development.</p>
7.7.4	Policies	
	<i>These policies supplement those set out in Section 7.4.</i>	
7.7.4.1	<p><i>That the Commercial Zone be applied to areas which are traditional commercial centres, and also to areas where the provision of commercial activity would not have adverse environmental effects and</i></p>	<p>The sites are zoned Commercial and within close proximity to the traditional commercial centre.</p> <p>The proposed activities are located within an area where the provision of visitor</p>

	would contribute to the needs and well-being of the community.	accommodation is predominant and in close proximity to retail activities. The proposed activities are complementary to the tourism-orientated services operating within the Commercial zone.
7.7.4.2	That the range of activities provided for in the Commercial Zone be limited only by the needs for the effects generated by the particular activity to be consistent with other activities in the zone.	Noted. The activities proposed are consistent with those existing within the locality and the tourism-orientated nature of the services provided for within the zone.
7.7.4.3	That standards be applied that protect visual and environmental amenity within the Commercial Zone, and the amenity of adjacent zones.	<p>A low-rise development is proposed and amenity planting along the road frontage will be provided.</p> <p>The cabins will be located centrally within the site to provide for on-site amenity and to maintain amenity values and have been designed and located to minimise earthworks and orientated to protect visual and environmental amenity within the zone and amenity of adjacent zones.</p>
7.7.4.4	That stormwater disposal systems do not result in suspended solids, industrial by-products, oil, or other contaminated substance or waste entering the stormwater collection system in concentrations that are likely to pose an immediate or long term hazard to human health or the environment.	An on-site stormwater management system is proposed.
7.7.5 - Zone Rules		
	<p>Activities in the Commercial Zone must comply not only with the zone rules but also with the relevant rules in Part 3 of the Plan - District Wide Provisions. An activity may be permitted by the zone rules but may require a resource consent because it does not comply with one or more of the rules in Part 3.</p> <p>Particular attention is drawn to:</p> <p>(a) Chapter 12 Natural and Physical Resources (and the District Plan Maps);</p> <p>(b) Chapter 13 Subdivision;</p> <p>(c) Chapter 14 Financial Contributions;</p> <p>(d) Section 15.1 Traffic, Parking and Access;</p> <p>(e) Chapter 16 Signs and Lighting;</p> <p>(f) Chapter 17 Designations and Utility Services (and the Zone Maps).</p>	
7.7.5.1	Permitted Activities	
	<p>An activity is a permitted activity in the Commercial Zone if:</p> <p>(a) it complies with the standards for permitted activities set out in Rules 7.7.5.1.1 to 7.7.5.1.12 below; and</p> <p>(b) it complies with the relevant standards for permitted activities set out in Part 3 of the Plan - District Wide Provisions.</p>	<p>Does not comply with Rule 7.7.5.1.4 and therefore is a Restricted Discretionary Activity under Rule 7.7.5.3.</p>

7.7.5.1.1	Maximum Building Height: 10m	Complies
7.7.5.1.2	Sunlight	Not applicable adjacent sites are zoned Commercial.
7.7.5.1.3	Visual Amenity and Environment Protection	Not applicable adjacent sites are zoned Commercial.
7.7.5.1.4	Setback from Boundaries ‘Pedestrian Frontage’: All buildings to be built up to the road boundary and a verandah provided along the full frontage of the site that relates to neighbouring site to provide continuous pedestrian cover.	Does not comply
7.7.5.1.5	Noise Mitigation for Residential Activities: <i>Noise Mitigation for Residential Activities: Acoustic design report to be provided. Attenuation of noise between any boundary and living room is no less than 20dB and between any boundary and any room used for sleeping is no less than 30dB.</i>	Will comply Consent condition.
7.7.5.1.6	Transportation <i>Refer Chapter 15 – Transportation for Traffic, Parking and Access rules</i>	Will comply
7.7.5.1.7	Keeping of Animals <i>No site shall be used for factory farming, a boarding or breeding kennel or a cattery.</i>	Complies
7.7.5.1.8	Noise <i>All activities within the zone shall be conducted so that noise measured at any point within any other site in the zone shall not exceed: 0700 to 2200 hours 65 dBA L₁₀ 2200 to 0700 hours 55 dBA L₁₀ and 80 dBA L_{max}.</i>	Will comply Consent condition.
7.7.5.1.10	Roof Pitch	Not applicable
7.7.5.1.11	Stormwater <i>The disposal of collected stormwater from the roof of all new buildings and new impervious surfaces provided that the activity is within an existing consented urban stormwater management plan or discharge consent.</i>	Complies Refer Stormwater Management report in Attachment 4 .
7.7.5.1.12	Helicopter Landing Area <i>A helicopter landing area shall be at least 200m from the nearest boundary of any of the Residential, Coastal Residential, Russell Township or Point Veronica Zones.</i>	Not applicable as a helicopter landing pad is not proposed.
7.7.5.2	Controlled Activities	
	<i>An activity is a controlled activity in the Commercial Zone if:</i>	

	<p>(a) it complies with all of the standards for permitted activities except for any one of the following Rules 7.7.5.1.2 Sunlight, 7.7.5.1.6 Transportation and 7.7.5.1.11 Stormwater above; and</p> <p>(b) it complies with Rules 7.7.5.2.1 Sunlight, 7.7.5.2.2 Transportation and Rule 7.7.5.2.3 Stormwater below; and</p> <p>(c) it complies with the relevant standards for permitted or controlled activities set out in Part 3 of the Plan - District Wide Provisions.</p> <p>The Council must approve an application for a land use consent for a controlled activity, but it may impose conditions on that consent.</p>	
7.7.5.2.1	Sunlight	Not applicable
7.7.5.2.2	Transportation Refer to Chapter 15 – Transportation for Traffic, Parking and Access rules.	Will comply
7.7.5.2.3	<p>Stormwater The disposal of collected stormwater from the roof of all new buildings and new impervious surfaces provided that:</p> <p>(a) where the means of disposal of collected stormwater will be by way of piping to an approved outfall, each allotment shall be provided with a piped connection to the outfall laid at least 600mm into the net area of the allotment. This includes land allocated on a cross-lease; and</p> <p>(b) the stormwater collection system shall be designed to avoid any contaminants stored or used on the site from being entrained in any stormwater discharge unless that stormwater is discharged through a stormwater interceptor system; and</p> <p>(c) the site is managed such that the concentration of contaminants in stormwater leaving the site do not pose an immediate or long term hazard to human health or the environment.</p> <p>Note: In order to meet the conditions listed above it is strongly recommended that the stormwater collection system be designed in accordance with the onsite volume control practices as contained in “Technical Publication 10, Stormwater Management Devices – Design Guidelines Manual”, Auckland Regional Council (2003).</p>	<p>Will Comply Refer to the recommendations contained in the Stormwater Management report (refer Attachment 4).</p>
7.7.5.3	Restricted Discretionary Activities	
	An activity is a restricted discretionary activity in the Commercial Zone if:	

	<p>(a) it does not comply with any one of the following Rules 7.7.5.1.1 Building Height; 7.7.5.1.3 Visual Amenity and Environmental Protection; 7.7.5.1.4 Setback from Boundaries; 7.7.5.1.5 Noise Mitigation for Residential Activities; 7.7.5.1.8 Noise; 7.7.5.2.1 Sunlight and/or 7.7.5.2.2 Transportation; as set out above; but</p> <p>(b) it complies with all of the other rules for permitted and controlled activities under Rules 7.7.5.1 and 7.7.5.2; and</p> <p>(c) it complies with Rules 7.7.5.3.1 Building Height; 7.7.5.3.2 Transportation; 7.7.5.3.3 Visual Amenity & Environmental Protection; 7.7.5.3.4 Setback from Boundaries; 7.7.5.3.5 Noise Mitigation for Residential Activities; 7.7.5.3.6 Noise and 7.7.5.3.7 Sunlight below; and</p> <p>(d) it complies with the relevant standards for permitted, controlled or restricted discretionary activities set out in Part 3 of the Plan - District Wide Provisions.</p> <p>The Council may approve or refuse an application for a restricted discretionary activity, and it may impose conditions on any consent.</p> <p>In assessing an application for a restricted discretionary activity, the Council will restrict the exercise of its discretion to the specific matters listed for each rule below, or where there is no rule, to the specific matters listed below under the appropriate heading.</p>	
7.7.5.3.1	Maximum Building Height: 10m	Complies
7.7.5.3.2	Transportation Refer to Chapter 15 – Transportation for Traffic, Parking and Access rules.	Complies
7.7.5.3.3	Visual Amenity and Environment Protection	Not applicable as the adjacent sites are zoned Commercial.
7.7.5.3.4	Setback from Boundaries ‘Pedestrian Frontage’:	Restricted Discretionary Activity
	<p>In assessing an application resulting from a breach of Rule 7.7.5.1.4 Setback from Boundaries the matters to which the Council will restrict its discretion are:</p> <p>(a) where a pedestrian frontage is required, the extent to which the proposal is in keeping with the existing character and form of the street or road, in particular with the external scale, proportions and buildings on the site and on adjacent sites;</p> <p>(b) the extent to which the buildings and their use will impact on the public use and enjoyment of adjoining esplanade reserves and strips and adjacent coastal marine areas.</p>	Refer assessment provided below.
7.7.5.3.5	Noise Mitigation for Residential Activities	Will comply Consent condition.
7.7.5.3.6	Noise	Will comply Consent condition.
7.7.5.3.7	Sunlight	Not applicable

Part 3 District-Wide Provisions		
Chapter 12.2 Indigenous Flora and Fauna		
12.2.6.1.1	<p>Indigenous Vegetation Clearance <i>Notwithstanding any rule in the Plan to the contrary but subject to Rules 12.5.6.1.1, 12.5.6.1.3 and 12.5.6.2.2 in the Heritage section of this Plan, indigenous vegetation clearance is permitted throughout the District where the clearance is for any of the following purposes:</i></p> <p>(o) <i>it involves the felling, trimming, damaging or removal of a tree or group of trees in an urban environment unless the tree or group of trees is—</i></p> <p style="padding-left: 40px;">(A) <i>specifically identified in the plan (refer to Chapter 12.5 and Appendix 1D);</i> or (B) <i>located within an area in the district that—</i></p> <p style="padding-left: 80px;">(i) <i>is a reserve (within the meaning of section 2(1) of the Reserves Act 1977); or</i> (ii) <i>is subject to a conservation management plan or conservation management strategy prepared in accordance with the Conservation Act 1987 or the Reserves Act 1977.</i></p>	<p>Complies</p> <p>The site does not contain any heritage features as detailed in Chapter 12 <i>Heritage</i>.</p> <p>The site does not contain indigenous vegetation that is specifically identified in the Plan, nor within a reserve or subject to a conservation management plan or a conservation management strategy.</p>
Chapter 12.4 Natural Hazards		
12.4.6.1.2	<p>Fire Risk to Residential Dwellings</p> <p>(a) <i>Residential units shall be located at least 20m away from the drip line of any trees in a naturally occurring or deliberately planted area of scrub or shrubland, woodlot or forest;</i></p> <p>(b) <i>Any trees in a deliberately planted woodlot or forest shall be planted at least 20m away from any urban environment zone, Russell Township or Coastal Residential Zone boundary, excluding the replanting of plantation forests existing at July 2003.</i></p>	<p>Complies</p> <p>The proposed residential units are not located at least 20m from the drip line of any trees.</p>
Chapter 15 Transportation		
15.1.6A.1.	<p>Maximum Daily One-Way Traffic Movements <i>Commercial Zone:</i></p> <p>200 – Permitted Activity 201-500 – Controlled Activity</p>	<p>Permitted Activity</p> <p>Less than 200 maximum daily one-way traffic movements. The Traffic Impact Assessment anticipates that a maximum of 74 daily traffic movements would be generated by the proposed activities.</p>

	More than 500 – Restricted Discretionary Activity	
15.1.6A.2.1	<p>Traffic Intensity</p> <p><i>Residential:</i> Standard Residential Unit – 10 per unit Home Unit/Town House – 7 per unit/house</p> <p><i>Casual Accommodation:</i> Camping Grounds/Motor Camps: 3 per unit and/or two per camp site.</p>	<p>Complies</p> <p>3 x 5 units = 15 2 x 5 campervans = 10 1 x dwelling (existing) = 10</p> <p>Maximum: 35</p>
15.1.6B	<p>Parking</p> <p><i>Residential:</i> Standard Residential Unit – 2 per unit Home Unit/Town House – 2 per unit</p> <p><i>Casual Accommodation:</i> Camping Grounds/Motor Camps: 1 per unit/camp site, plus 1 per two employees.</p> <p><i>Accessible Car Parking Spaces:</i> Where 20 or less car parking spaces provided – then one accessible car parking space is to be provided. Where 21 – 50 car parking spaces are provided – then two accessible car parking spaces are to be provided.</p> <p><i>Loading Spaces:</i> Building GFA no greater than 200m² – no loading space is required to be provided. Building GFA between 200m² and 500m² – one loading space to be provided. Building GFA between 500m² and 5000m² – two loading spaces are to be provided. Building GFA exceeding 5000m² – three loading spaces are to be provided.</p> <p>All parking, loading and access drives and manoeuvring areas are to be formed and provided with an all-weather surface, drained and marked out.</p>	<p>Complies</p> <p>Existing dwelling = 2 parking spaces</p> <p>5 units plus 5 campervan sites = 10 x 1 per site = 10</p> <p>Two employees = 1</p> <p>13 parking spaces.</p> <p>Fourteen parking spaces proposed including one accessible parking space.</p> <p>No loading space is required as each site will contain a building gross floor area of less than 200m².</p> <p>Metalled surface proposed across the parking, loading and access drives within the site that will provide for drainage and will be marked out.</p>
15.1.6C	<p>Access</p> <p>A private access may serve a maximum of 8 household equivalents.</p> <p>One-way private accessway from the road – not less than 3m or more than 4m in width and have a minimum overhead clearance of 4.2m.</p>	<p>Each site has an existing vehicle crossing. The vehicle crossing to 32 Selwyn Road will be upgraded to comply with the Far North District Council Engineering Standards for Vehicle Crossings.</p>

	<p>Two-way private accessway from the road – shall not be less than 6m or more than 7m in width and have a minimum overhead clearance of 4.2m.</p> <p>Maximum width of vehicle access over footpaths: 6m. Maximum of two crossings per site.</p> <p>Accessway to be no steeper than 1:20 adjacent to the road boundary for a length of at least 6m.</p> <p>Stormwater management required.</p> <p>Private access of streets in the urban zones the vehicle crossing is to be constructed in accordance with the Council’s <i>Engineering Standards and Guidelines</i> (June 2004 – Revised 2009).</p>	<p>Maximum width of the vehicle crossings is 4.5m.</p> <p>Complies</p> <p>Complies</p> <p>An on-site stormwater management system will be provided.</p> <p>Will comply The vehicle crossing to 32 Selwyn Road will be upgraded to comply with the Council’s <i>Engineering Standards and Guidelines</i> (June 2004 – Revised 2009), noting that there is a revised version 0.6 dated 2023. As the proposed activities are small in scale and nature and associated with a residential land use within the site, the application includes the construction of a vehicle crossing that has been designed to comply with the Engineering Standards and Guidelines for Residential Vehicle Crossings in Version 0.6 2023.</p>
Chapter 16 Signs and Lighting		
<p>16.6.1.2</p>	<p>General Requirements for all Signs <i>Commercial Zone</i></p> <p>Maximum height: (including support structure): the maximum height of any signs attached to, or displayed on a building or veranda may exceed 4m but shall not exceed the height of the building or otherwise provided for in Rules 16.6.1.4 or 16.6.1.5.</p> <p>Maximum Number of Freestanding Signs: no limits or otherwise restricted or provided by in Rules 16.6.1.4 or 16.6.1.5.</p>	<p>Will comply The maximum height of the proposed freestanding sign is 3.6m inclusive of the supporting structure.</p>
<p>16.6.1.3</p>	<p>Maximum Sign Area per Site <i>Commercial Zone:</i> For each road frontage: Less than 24m: 6m² Greater than 24m: 0.25m² for every 1m of road frontage, up to 12m².</p>	<p>Will comply.</p> <p>Area: 4.32m²</p>

Activity Status:

Proposed Visitor Accommodation - **Restricted Discretionary Activity** for non-compliance with Rule 7.7.5.1.4 as the buildings are not proposed to be located along the frontage of the site and no verandahs along the frontage of the site are proposed.

Assessment – Restricted Discretionary Activity:

7.7.5.3.4	Setback from Boundaries ‘Pedestrian Frontage’:	
	<p><i>In assessing an application resulting from a breach of Rule 7.7.5.1.4 Setback from Boundaries the matters to which the Council will restrict its discretion are:</i></p> <p><i>(a) where a pedestrian frontage is required, the extent to which the proposal is in keeping with the existing character and form of the street or road, in particular with the external scale, proportions and buildings on the site and on adjacent sites;</i></p>	<p>A land use consent for a five year duration is sought for the proposed activities.</p> <p>At present, the dwelling at 34 Selwyn Road is set back from the site boundaries.</p> <p>There is 58m² shop located at 30 Selwyn Road that is located adjacent to front boundary and contains a small canopy that extends along the frontage of the building.</p> <p>The adjacent site at 28 Selwyn Road is used for visitor accommodation and it is noted that this building is set back from the road and does not provide a covered verandah cover along the frontage of site.</p> <p>At present, properties accessed via the northern extent of Selwyn Road are used for residential activities, are set back from the road and do not provide verandah cover along the road frontage of the site.</p> <p>Overall, the application seeks consent for a limited duration of five years and the proposed activities are consistent with the existing environment along the northern extent of Selwyn Road, where visitor accommodation predominates. The proposal does not preclude the provision of verandahs being provided along the frontages of these sites as part of the proposed comprehensive development of these sites in the future.</p> <p>To mitigate the proposed parking areas located within the sites, landscape planting is proposed along the frontage of these sites and around the proposed cabins to maintain</p>

	<p><i>(b) the extent to which the buildings and their use will impact on the public use and enjoyment of adjoining esplanade reserves and strips and adjacent coastal marine areas.</i></p>	<p>amenity and to positively contribute to the streetscape.</p> <p>Not applicable.</p>
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Chapter 15 - Transportation		
15.1.3	Objectives	
15.1.3.1	To minimise the adverse effects of traffic on the natural and physical environment.	Appropriate measures are proposed be implemented to mitigate adverse traffic effects.
15.1.3.2	To provide sufficient parking spaces to meet seasonal demand in tourist destinations.	The proposal includes the provision of parking spaces to meet the seasonal demand.
15.1.3.3	To ensure that appropriate provision is made for on-site car parking for all activities, while considering safe cycling and pedestrian access and use of the site.	Appropriate provision has been made for the provision of on-site carparking to serve the proposed activities while considering safe cycling and pedestrian access and use of the site.
15.1.3.4	To ensure that appropriate and efficient provision is made for loading and access for activities.	Appropriate and efficient provision has been made for loading and access to the site for the proposed activities.
15.1.3.5	To promote safe and efficient movement and circulation of vehicular, cycle and pedestrian traffic, including for those with disabilities.	The design and layout of the parking spaces and associated access aisles promotes the safe and efficient movement and circulation of vehicular, cycle and pedestrian traffic, including for those with disabilities.
15.1.4	Policies	
15.1.4.1	That the traffic effects of activities be evaluated in making decisions on resource consent applications.	A Traffic Impact Assessment report has been prepared for the proposed activities (refer Attachment 5).
15.1.4.2	That the need to protect features of the natural and built environment be recognised in the provision of parking spaces.	<p>The proposal includes the provision of parking spaces on the flatter land located near the Selwyn Road frontage of the site to minimise earthworks required.</p> <p>Metalled parking areas will be provided to promote the on-site management of stormwater.</p>
15.1.4.3	That parking spaces be provided at a location and scale which enables the efficient use of parking spaces and handling of traffic generation by the adjacent roading network.	Appropriate provision has been made for the provision of on-site carparking to serve the proposed activities while considering safe cycling and pedestrian access and use of the site.

		The Traffic Impact Assessment report confirms that the proposed activities are able to be accommodated by the adjacent roading network.
15.1.4.4	That existing parking spaces are retained or replaced with equal or better capacity where appropriate, so as to ensure the orderly movement and control of traffic.	The proposal includes the provision of parking spaces to serve the proposed activities in a manner that will ensure the orderly movement and control of traffic.
15.1.4.5	That appropriate loading spaces be provided for commercial and industrial activities to assist with the pick-up and delivery of goods.	The proposal includes the provision of relocatable cabins and a commercial parking area, such that the gross floor area within each site is less than 200m ² , such that a loading space is not required to be provided under Rule 15.1.6B.1.6(a).
15.1.4.6	That the number, size, gradient and placement of vehicle access points be regulated to assist traffic safety and control, taking into consideration the requirements of both the New Zealand Transport Agency and the Far North District Council.	The proposal includes the use of the existing vehicle crossings to these sites, and these will be upgraded to accommodate the traffic movements associated with the proposed activities.
15.1.4.7	That the needs and effects of cycle and pedestrian traffic be taken into account in assessing development proposals.	The site plan shows that appropriate provision has been made for the movement of pedestrians within the site.
15.1.4.8	That alternative options be considered to meeting parking requirements where this is deemed appropriate by the Far North District Council.	Noted. Compliance with the parking standards will be achieved.
15.1.6	Rules	
15.1.6A	Traffic	
15.1.6A.1	Maximum Daily One Way Traffic Movements <i>Urban Environment</i> <i>Commercial Zone</i> Permitted Activity – 200 Controlled Activity – 201 – 500 Restricted Discretionary Activity – More than 500	Complies The Traffic Impact Assessment report has assessed that the maximum daily one way traffic movements will be 74, and therefore the proposal falls to be a Permitted Activity .
15.1.6A.2	Permitted Activities	
	An activity is a permitted activity if: (a) it complies with the standards set out in Rule 15.1.6A.2.1 ; and (b) it complies with the relevant standards for permitted activities in the particular zone in which it is located set out in Part 2 of the Plan – Environment Provisions; and (c) it complies with all other relevant standards for permitted activities set out in Part 3 of the Plan - District Wide Provisions.	

15.1.6A.2.1	Traffic Intensity	
	<p><i>The Traffic Intensity threshold value for a site shall be determined for each zone by Table 15.1.6A.1 above. The Traffic Intensity Factor for a proposed activity (subject to the exemptions identified below) shall be determined by reference to Appendix 3A in Part 4.</i></p> <p><i>This rule only applies when establishing a new activity or changing an activity on a site. However, when considering a new activity or changing an activity, the Traffic Intensity Factor for the existing uses (apart from those exempted above) on site need to be taken into account in order to address cumulative effects.</i></p> <p><i>Exemptions: The first residential unit on a site, farming, forestry and construction traffic (associated with the establishment of an activity) are exempt from this rule.</i></p>	
Appendix 3A	Traffic Intensity Factors	
	Land Use Activity	Traffic Intensity Factor (based on average daily one-way vehicle movements)
	<p>Residential Standard Residential Unit</p> <p>Casual Accommodation Camping Grounds/Motor Camps Motel</p> <p>Retail Shops (Including TAB facilities)/Shopping Centres</p>	<p>10 per unit</p> <p>3 per unit and/or 2 per camp site 3 per unit</p> <p>50 per 100m² GBA</p>
	<p>A Traffic Impact Assessment (refer Attachment 5) has been prepared that includes calculations for the traffic movements associated with the proposed activities as follows:</p> <p>Five Tourist Cabins x 6 trips = 30 vehicle trips per day Seven Commercial Car Parking Space x 2 vehicle trips per day = 14 vehicle trips per day Five Overnight Camping Spaces for Self-Contained Campervans x 4 trips = 20 vehicle trips per day.</p> <p>Total estimated traffic movements per day = 64</p> <p><u>Existing Activities:</u> Existing dwelling = 10 Shop (58m²) = 29</p> <p>64 + 10 + 29 = 103 total daily vehicle movements, however under Rule 15.1.6A.2.1 the first residential dwelling on a site is exempt from the traffic intensity calculations, such that the daily total vehicle trips per day = 93. The proposed activities, therefore, comply with the traffic intensity standard for a Permitted Activity in the Commercial zone (maximum standard: 200).</p>	

Appendix 3B-1	Standards for Private Access	
	<p>Commercial Zone: 1 H.E – 3m carriageway width and a maximum gradient of 1:8 unsealed.</p> <p>2 – 4 H.E's – 6m carriageway width Maximum Gradient: 1:5 sealed Stormwater Drainage measures required such that adverse effects are not created on adjoining properties or the public road, in accordance with Council's "Engineering Standards and Guidelines" (June 2004 - Revised - 2009).</p>	<p>The application plans show the upgrading works proposed to the vehicle crossing and vehicle access to 32 Selwyn Road that will provide access to the five self-contained campervan parking spaces and to the seven daily tariff commercial car parking spaces (34 vehicle trips per day).</p>
Appendix 3C	Parking Spaces Required	
	<p>Land Use Activity</p> <p>Residential Standard Residential Unit</p> <p>Casual Accommodation Camping Grounds/Motor Camps Motel</p> <p>Retail Shops (Including TAB facilities)/Shopping Centres</p>	<p>2 per unit</p> <p>1 per unit/camp site, plus 1 per 2 employees 1 per unit, plus 1 per 2 employees</p> <p>1 per 25m² GBA</p>
Appendix 3D	Manoeuvring and Parking Space Dimensions	
	<p>The application plans include dimensions for the proposed car and campervan parking spaces and manoeuvring spaces.</p>	
Appendix 3E	Vehicle Tracking Curves	
	<p>Tracking curves are shown on the application plans.</p>	



HODGSON
PLANNING CONSULTANTS

Far North District Plan Assessment

Paihia Terraces Ltd
30 to 34 Selwyn Road,
Paihia

HPC Reference PT2419

December 2024

Proposed Far North District Plan Assessment

Proposed Far North District Plan 2022

Applicant:	Paihia Terraces Ltd
Site Address:	30, 32, 34 Selwyn Road, Paihia
Legal Description:	Lot 1 DP 328135, RT NA114614. 829m ² Lot 2 DP 328135, RT NA114615, 691m ² Lot 3 DP 328135, NA114616) 760m ²

Proposed Far North District Plan, Notified 2022

Zone:	Mixed Use
Sub-Zone:	Area B
Zone Overlay:	Pedestrian Frontage
Environment:	Coastal

Proposal: Land Use Consent for a maximum duration of five years to establish the following:

- five relocatable cabins to provide for visitor accommodation;
- seven commercial car parking spaces (daily tariff);
- Overnight parking for up to five self-contained campervans;
- Associated signage, landscaping, earthworks and infrastructure required to establish the above activities within these sites.

Proposed Far North District Plan, Notified 2022

Zone:	Mixed Use
Overlay:	Coastal Environment
Building Height Control:	Height Limit: Area B
Specific Controls:	Pedestrian Frontage

Mixed Use Zone

Overview:

The district's urban business centres have traditionally been zoned commercial and contain retail activities, commercial services, food and beverage establishments as well as social and educational services, with limited residential activities.

The Mixed Use zone provides a framework in which commercial and residential activities can co-exist and it enables a range of compatible activities. The focus of the zone is to revitalise urban centres and support business owners, residents and visitors, while ensuring that associated effects are appropriately managed. The Mixed Use zone will contribute to the vibrancy, safety and prosperity of the district's urban centres and will be serviced by appropriate infrastructure.

The Council has a responsibility under the RMA, the National Policy Statement on Urban Development and the RPS to ensure that there is sufficient land for housing and business to meet the future demands of the district, that development occurs in the right location and that it is appropriately serviced.

Objectives		
MUZ-01	<i>The Mixed Use zone is the focal point for the district's commercial, community and civic activities, and provides for residential development where it complements and is not incompatible with these activities.</i>	The proposal includes the provision of casual accommodation for tourists and overnight parking for self-contained campervans within the sites to cater for the tourists that visit the area. The consent is for a five year duration, then it is intended that the site will be redeveloped in accordance with the direction outlined in the Proposed District Plan.
MUZ-02	<i>Development in the Mixed Use zone is of a form, scale, density and design quality that contributes positively to the vibrancy, safety and amenity of the zone.</i>	The sites are currently underutilised and located within close proximity to the retail centre. Visitor accommodation and commercial parking is proposed that will cater for tourists visiting the area. The sites are located within close proximity to the town centre contributing towards the vibrancy, safety and amenity of the zone.

		<p>The consent is for visitor accommodation and commercial parking activities for a maximum duration of five years. Then the applicant intends to undertake a redevelopment of the property (across the three sites) in a manner consistent with the direction contained in the Proposed District Plan.</p> <p>Amenity planting along the road frontage will be provided and on-site services made available.</p>
MUZ-03	<i>Enable land use and subdivision in the Light Industrial zone where there is adequacy and capacity of available or programmed development infrastructure to support it.</i>	Not applicable.
MUZ-04	<i>The adverse environmental effects generated by activities within the zone are managed, in particular at zone boundaries.</i>	<p>The site is located within the Mixed Use Zone and is not located near the zone boundaries.</p> <p>The design and layout of the proposed development within the site ensures that adverse effects are managed within the site including the on-site management of stormwater.</p> <p>Appropriate on-site services will be made available and proposed consent conditions will ensure adverse environmental effects are appropriately managed.</p>
MUZ-05	<i>Residential activity in the Mixed Use zone is located above commercial activities to ensure active street frontages, except where the interface is with the Open Space zone.</i>	<p>The proposal seeks consent to establish five relocatable cabins and five sites to accommodate overnight parking of self-contained campervans for a maximum duration of five years.</p> <p>Given the temporary nature of the activities, it is considered that the proposal will not be contrary to this objective.</p> <p>The site contains an existing shop and a dwelling that will be retained. The surrounding sites all currently contain visitor accommodation at ground level, so the proposed activities are not inconsistent with the established land uses within the immediate locality/zone.</p>
Policies		
MUZ-P1	<i>Enable a range of commercial, community, civic and residential activities in the Mixed Use zone where:</i>	The proposal to establish five relocatable cabins and five parking spaces for the overnight parking of self-contained campervans and

	<p>a. <i>it supports the function, role, sense of place and amenity of the existing environment; and</i></p> <p>b. <i>there is:</i></p> <p>i. <i>existing infrastructure to support development and intensification, or</i></p> <p>ii. <i>additional infrastructure capacity can be provided to service the development and intensification.</i></p>	<p>seven commercial car parking spaces. These activities have been designed and located to support the function, role and amenity of the existing environment.</p> <p>The activities can be served by existing infrastructure and on-site stormwater management measures will be implemented as recommended in the Stormwater Management Report (refer Attachment 4) including the upgrading of the stormwater pipe that connects the site to the public stormwater reticulation network.</p>
MUZ-P2	<p><i>Require all subdivision in the Mixed Use zone to provide the following reticulated services to the boundary of each lot:</i></p> <p>a. <i>telecommunications:</i></p> <p>i. <i>fibre where it is available;</i></p> <p>ii. <i>copper where fibre is not available;</i></p> <p>iii. <i>copper where the area is identified for future fibre deployment.</i></p> <p>b. <i>local electricity distribution network; and wastewater, potable water supply and stormwater where they are available.</i></p>	<p>Not applicable. Subdivision is not proposed.</p>
MUZ-P3	<p><i>Require development in the Mixed Use zone to contribute positively to:</i></p> <p>a. <i>high quality streetscapes;</i></p> <p>b. <i>pedestrian amenity;</i></p> <p>c. <i>safe movement of people of all ages and abilities;</i></p> <p>d. <i>community well-being, health and safety; and</i></p> <p>e. <i>traffic, parking and access needs.</i></p>	<p>Amenity planting along the streetscape will be provided to contribute towards the streetscape.</p> <p>The location and orientation of the cabins promote passive surveillance of the site and street to promote the safe movement of people and pedestrians and public safety.</p> <p>Infrastructure will be made available to serve the proposed development including the on-site management of stormwater. The provision of a metalled parking area utilising the existing vehicle crossings to the site and will provide for the safe movement of people and for traffic parking and access needs.</p> <p>One accessible car parking space is proposed to be provided.</p>
MUZ-P4	<p><i>Require development in the Mixed Use zone that is adjacent to</i></p>	<p>The site is not located adjacent to Residential and Open Space zones.</p>

	<p><i>Residential and Open Space zones to maintain the amenity values of those areas, having specific regard to:</i></p> <ol style="list-style-type: none"> <i>a. visual dominance;</i> <i>b. privacy;</i> <i>c. shadowing;</i> <i>d. ambient noise; and</i> <i>e. light spill.</i> 	<p>The proposed relocatable cabins are single storey and will be staggered within the site and setback from the boundaries of the site to provide for privacy and to avoid adverse effects associated with visual dominance, shadowing, ambient noise and light spill.</p>
<p>MUZ-P5</p>	<p><i>Restrict activities that are likely to have an adverse effect on the function, role, sense of place and amenity of the Mixed Use zone, including:</i></p> <ol style="list-style-type: none"> <i>a. residential activity, retirement facilities and visitor accommodation on the ground floor of buildings, except where a site adjoins an Open Space zone;</i> <i>b. light or heavy industrial activity;</i> <i>c. storage and warehousing;</i> <i>d. large format retail activity over 400 m²; and</i> <i>e. waste management activity.</i> 	<p>The proposal seeks consent for five relocatable cabins to be located and used to provide visitor accommodation and five parking spaces for overnight stays for self-contained campervans within the site. These activities are consistent with the predominance of visitor accommodation provided at ground level that presently exists along the northern extent of Selwyn Road.</p> <p>The provision of seven commercial parking spaces within the site is consistent with the existing character and activities within the locality. The parking spaces will provide for the needs of visitors to the town centre.</p> <p>Given that the front portion of these sites are currently vacant, the proposed activities will contribute to a sense of vibrancy and place in a manner that will not adversely affect the function, role and amenity of the zone. Consent is sought for a maximum duration of five years.</p> <p>The proposal will enable the site to be redeveloped in the future in accordance with the direction contained in the Proposed District Plan (which is currently at the hearings stage with Council decisions scheduled to be made by 27 May 2026).</p> <p>The proposal has been designed to minimise earthworks and utilise existing vehicle crossings and services.</p> <p>Amenity planting along the road frontage is proposed and the cabins will be orientated towards the street providing an active frontage and opportunities for passive surveillance that will contribute towards public safety and vibrancy of the town centre.</p>

<p>MUZ-P6</p>	<p><i>Promote energy efficient design and the use of renewable electricity generation in the construction of mixed use development.</i></p>	<p>The proposal seeks to utilise existing vehicle crossings to the site and minimise earthworks.</p> <p>The cabins have been orientated towards the street and for solar gain.</p>
<p>MUZ-P7</p>	<p><i>Consider the following effects when assessing applications to establish residential, early childhood, retirement and education facilities:</i></p> <ol style="list-style-type: none"> <i>a. the level of ambient noise;</i> <i>b. reduced privacy;</i> <i>c. shadowing and visual domination; and</i> <i>d. light spill.</i> 	<p>The proposed single storey relocatable cabins will be located centrally within the site, providing a separation distance to buildings located on adjacent sites, minimising earthworks and avoiding adverse shadowing, visual dominance, light spill and privacy effects.</p> <p>A condition of consent will require that noise from the proposed activities comply with the noise standards for the zone at all times.</p>
<p>MUZ-P8</p>	<p><i>Manage land use and subdivision to address the effects of the activity requiring resource consent, including (but not limited to) consideration of the following matters where relevant to the application:</i></p> <ol style="list-style-type: none"> <i>a. consistency with the scale, density, design, amenity and character of the mixed use environment;</i> <i>b. the location, scale and design of buildings or structures, outdoor storage areas, parking and internal roading;</i> <i>c. at zone interfaces:</i> <ol style="list-style-type: none"> <i>i. any setbacks, fencing, screening or landscaping required to address potential conflicts;</i> <i>ii. any adverse effects on the character and amenity of adjacent zones;</i> <i>d. the adequacy and capacity of available or programmed development infrastructure to accommodate the proposed activity; including:</i> <ol style="list-style-type: none"> <i>i. opportunities for low impact design principles;</i> <i>ii. management of three waters infrastructure and trade waste;</i> <i>e. managing natural hazards;</i> <i>f. the adequacy of roading infrastructure to service the proposed activity;</i> <i>g. any adverse effects on historic heritage and cultural values, natural features and landscapes or indigenous biodiversity, and</i> 	<p>A land use consent for a maximum duration of five years is sought for the establishment and use of five relocatable cabins to be used to provide visitor accommodation, five parking spaces for overnight stays for self-contained campervans and seven commercial car parking spaces.</p> <p>These activities are consistent with the land use activities occurring within the immediate locality avoiding adverse amenity and character effects.</p> <p>The proposed scale and density of development is minor given that the cabins and parking areas will be located within a site with a combined area of 2280m². The buildings will be located centrally within the site providing a separation distance to adjacent sites avoiding potential adverse effects upon neighbouring sites.</p> <p>The proposal includes the provision of infrastructure to service the proposed activities and on-site parking and manoeuvring provided.</p> <p>An on-site stormwater management system is proposed, and a metalled parking surface will promote on-site soakage and minimise the extent of impervious surfaces within the site. This will avoid potential adverse downstream flooding effects.</p> <p>Amenity planting is proposed along the site frontage that will contribute positively towards the streetscape and amenity.</p>

	<p><i>h. any historical, spiritual, or cultural association held by tangata whenua, with regard to the matters set out in Policy TW-P6.</i></p>	<p>The proposal has been designed to minimise the extent of earthworks required and maintain the natural landform.</p> <p>The existing vehicle crossings to the sites will be used, parking areas formed, and signage provided to direct visitors to the site.</p> <p>A Traffic Impact Assessment has been prepared for the proposal that confirms that the activities are able to be accommodated by the existing roading network and that an appropriate number of parking spaces to support the proposed activities will be provided.</p> <p>There are no known sites of significance recorded for the property. The property is not located within an area with historic heritage and cultural values, natural features and landscapes or indigenous biodiversity.</p>
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General District-Wide Matters

The sites are located within the Coastal Environment as shown on the Planning Maps.

Coastal Environment

Overview

The Far North District has a vast and complex coastal environment with dynamic natural processes, unique natural and physical attributes and high cultural values. The District Plan has mapped the coastal environment and identifies areas within it that contain high or outstanding natural character. These areas were originally identified through the regional mapping project undertaken by the Regional Council for the RPS. The methodology for identifying them can be found in APP1 - Mapping methods and criteria and the schedules of high and outstanding natural character can be found in SCHED7 and SCHED8 of the District Plan. The mapped coastal environment accounts for approximately 12% of the district's total land area.

Much of the district's coastline is relatively undeveloped in the sense that there is limited built development and supporting infrastructure. The past few of decades have seen an increasing pressure for development in coastal areas, particularly along the east coast where there is a continued pattern of settlement which has placed additional pressure on coastal resources and character. Consideration needs to be given to both the preservation of the natural character of the coastal environment and the level of intervention to manage land use and subdivision, while ensuring the community's health, safety and wellbeing.

The coastal hazard rules are located in this chapter in accordance with the Planning Standards, while other natural hazards such as flooding are controlled in the Natural Hazards chapter. The Natural Hazards chapter consolidates all of the objectives and policies related to natural hazards including rules that must be considered when assessing proposals within a Coastal Hazard Area.

Council has a responsibility under the RMA, the NZCPS and the RPS to preserve and protect the natural character of the coastal environment from inappropriate land use and subdivision.

Objectives		
CE-O1	<i>The natural character of the coastal environment is identified and managed to ensure its long-term preservation and protection for current and future generations.</i>	<p>The proposal includes temporary activities (up to five years) within the site and includes relocatable buildings that have been sited to minimise earthworks to retain the natural character and landform of the site.</p> <p>The location and establishment of five single storey cabins, each with a maximum area of 30.6m² within the site will provide for visitor accommodation. These buildings are low rise, will be located centrally within the site and are able to be serviced.</p>

		The proposal is considered to be consistent with this objective.
CE-O2	<p><i>Land use and subdivision in the coastal environment:</i></p> <p>a. <i>preserves the characteristics and qualities of the natural character of the coastal environment;</i></p> <p>b. <i>is consistent with the surrounding land use;</i></p> <p>c. <i>does not result in urban sprawl occurring outside of urban zones;</i></p> <p>d. <i>promotes restoration and enhancement of the natural character of the coastal environment; and</i></p> <p>e. <i>recognises tangata whenua needs for ancestral use of whenua Māori.</i></p>	<p>The proposal minimises earthworks, to maintain the natural landform.</p> <p>The proposed activities are small scale and of a temporary duration, and the provision of visitor accommodation is consistent with the predominance of visitor accommodation established within the immediate locality. The proposed activities are consistent with the surrounding land use and will not result in urban sprawl.</p> <p>Amenity planting along the street frontage is proposed that will contribute positively towards the streetscape, amenity and character of the zone.</p> <p>It is considered that the proposal is consistent with this objective.</p>
CE-O3	<p><i>Land use and subdivision in the coastal environment within urban zones is of a scale that is consistent with existing built development.</i></p>	<p>The proposal seeks to establish five single storey relocatable cabins, each with a floor area of 30.6m² within the site. The cabins will be used to provide for visitor accommodation which is a well-established and dominant land use within the immediate locality of the site.</p> <p>The provision of five parking spaces for the overnight parking of self-contained campervans and a commercial car parking area with seven spaces is proposed. These areas will be metalled and marked out and consistent with the extent of parking areas provided for visitor accommodation located within the locality.</p> <p>These activities are of a small scale and limited duration, such that the site can readily be redeveloped to a scale consistent with that anticipated within the zone.</p>
Policies		
CE-P1	<p><i>Identify the extent of the coastal environment as well as areas of high and outstanding natural character using the assessment criteria in APP1-Mapping methods and criteria.</i></p>	Noted.
CE-P2	<p><i>Avoid adverse effects of land use and subdivision on the characteristics and</i></p>	Complies as the site is not located within an area of outstanding natural character, nor located within or contain an ONL or ONF.

	<p><i>qualities of the coastal environment identified as:</i></p> <p>a. <i>outstanding natural character;</i></p> <p>b. <i>ONL;</i></p> <p>c. <i>ONF.</i></p>	
CE-P3	<p><i>Avoid significant adverse effects and avoid, remedy or mitigate other adverse effects of land use and subdivision on the characteristics and qualities of the coastal environment not identified as:</i></p> <p>a. <i>outstanding natural character;</i></p> <p>b. <i>ONL;</i></p> <p>c. <i>ONF.</i></p>	<p>The site is located within the commercial centre of Paihia and one block inland from the coastline.</p> <p>Minimal earthworks are proposed, and the cabins are relocatable (readily able to be removed from the site) and a metalled parking area will be provided to minimise stormwater runoff and impervious surfaces.</p> <p>The proposal has been designed to avoid significant adverse effects on the characteristics and qualities of the coastal environment, particularly noting that a five year duration of consent is sought.</p>
CE-P4	<p><i>Preserve the visual qualities, character and integrity of the coastal environment by:</i></p> <p>a. <i>consolidating land use and subdivision around existing urban centres and rural settlements; and</i></p> <p>b. <i>avoiding sprawl or sporadic patterns of development.</i></p>	<p>Complies. The site is located within an existing urban environment and avoids sprawl or sporadic patterns of development.</p> <p>The proposed land use is to be established on a site located within the existing urban centre. The proposed visitor accommodation and commercial parking are activities anticipated within the urban centre and zone.</p>
CE-P5	<p><i>Enable land use and subdivision in urban zones within the coastal environment where:</i></p> <p>a. <i>there is adequacy and capacity of available or programmed development infrastructure; and</i></p> <p>b. <i>the use is consistent with and does not compromise the characteristics and qualities.</i></p>	<p>The proposed activities are able to be serviced by infrastructure and an on-site stormwater management system is proposed.</p> <p>The proposed development will utilise existing vehicle crossings to the site.</p> <p>The proposed activities are consistent with land use activities within the locality and will not compromise the characteristics and qualities.</p> <p>The proposal seeks consent for a maximum duration of five years. This will allow for the site to be redeveloped consistent with the outcomes anticipated under the Proposed District Plan.</p>
CE-P6	<p><i>Enable farming activities within the coastal environment where:</i></p>	<p>Not applicable.</p>

	<p>a. the use forms part of the values that established the natural character of the coastal environment; or</p> <p>b. the use is consistent with and does not compromise the characteristics and qualities.</p>	
CE-P7	<p>Provide for the use of Māori Purpose zoned land and Treaty Settlement land in the coastal environment where:</p> <p>a. the use is consistent with the ancestral use of that land; and</p> <p>b. the use does not compromise any identified characteristics and qualities.</p>	Not applicable.
CE-P8	<p>Encourage the restoration and enhancement of the natural character of the coastal environment.</p>	<p>The proposal includes amenity planting along the street frontage.</p> <p>Minimal earthworks are proposed retain the natural landform and character of the site.</p>
CE-P9	<p>Prohibit land use and subdivision that would result in any loss and/or destruction of the characteristics and qualities in outstanding natural character areas.</p>	Complies. The proposed land use will not result in any loss and/or destruction of the characteristics and qualities in outstanding natural character areas.
CE-P10	<p>Manage land use and subdivision to preserve and protect the natural character of the coastal environment, and to address the effects of the activity requiring resource consent, including (but not limited to) consideration of the following matters where relevant to the application:</p> <p>a. the presence or absence of buildings, structures or infrastructure;</p> <p>b. the temporary or permanent nature of any adverse effects;</p> <p>c. the location, scale and design of any proposed development;</p> <p>d. any means of integrating the building, structure or activity;</p> <p>e. the ability of the environment to absorb change;</p> <p>f. the need for and location of earthworks or vegetation clearance;</p> <p>g. the operational or functional need of any regionally significant infrastructure to be sited in the particular location;</p> <p>h. any viable alternative locations for the activity or development;</p> <p>i. any historical, spiritual or cultural association held by tangata whenua, with</p>	<p>The site is located within an established urban environment, and able to be serviced with connections to water supply and wastewater. An on-site stormwater management system is proposed to address additional impervious surfaces within the site and avoid downstream flooding effects.</p> <p>The proposed seeks to utilise the existing vehicle crossings to the sites and a Traffic Impact Assessment report has been prepared that confirms that traffic is able to be accommodated by the roading network.</p> <p>The proposal seeks consent for a maximum duration of five years. The proposal includes the provision of visitor accommodation via five relocatable cabins within the site and five parking sites for overnight parking for self-contained campervans and seven commercial parking spaces. These can be readily removed to allow the site to be redeveloped in the future.</p> <p>The buildings are relocatable, comprising cabins that are single storey and 30.6m² in area. These will be staggered centrally within the site to minimise earthworks and</p>

	<p>regard to the matters set out in Policy TW-P6;</p> <p>j. the likelihood of the activity exacerbating natural hazards;</p> <p>k. the opportunity to enhance public access and recreation;</p> <p>l. the ability to improve the overall quality of coastal waters; and</p> <p>m. any positive contribution the development has on the characteristics and qualities.</p>	<p>orientated towards the road. Parking areas will be provided on the flatter land near the road and utilise existing vehicle crossings.</p> <p>Metalled surface will be provided to facilitate on-site management of stormwater and reduce the extent of impervious surfaces within the site. Amenity planting along the street frontage is proposed.</p>
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Earthworks

This chapter contains provisions that have legal effect.

Rules:

Rules		
EW-R1	<p>Earthworks for buildings or structures and extensions to buildings or structures</p> <p>Activity status: Permitted where:</p> <p>PER-1</p> <p>The earthworks complies with standards: EW-S1 Maximum earthworks thresholds; EW-S2 Maximum depth and slope; EW-S4 Site reinstatement; EW-S6 Setbacks; EW-S7 Land stability; EW-S8 Nature of filling material; and EW-S9 Flood and coastal hazards.</p>	<p>Activity status where compliance not achieved with PER-1: Restricted Discretionary</p> <p>Matters of Discretion are restricted to:</p> <p>a. the matters of discretion of any infringed standard.</p>

Standards:

Standards		Comments
EW-S1	<p>Maximum Earthworks Thresholds</p> <p>The following maximum volume and area thresholds for all earthworks undertaken on a site within a single calendar year in the Mixed Use Zone:</p> <p>Maximum Volume: 200m³ Area: 2500m²</p>	<p>The earthworks have been calculated across the property (three sites), such that the earthworks proposed within <u>each site</u> will comply with this standard.</p>
EW-S2	<p>Maximum Depth and Slope</p>	<p>The maximum depth of cut or height of fill will not exceed 1.5m.</p>

	<p><i>The maximum depth of any cut or height of any fill shall not exceed:</i></p> <ol style="list-style-type: none"> <i>1. 1.5m, i.e. maximum permitted cut and fill height may be 3m; or</i> <i>2. 3m subject to it being retained by an engineered retaining wall, which has had a building consent issued.</i> 	
<p>EW-S3 <i>*Immediate legal effect.</i></p>	<p>Accidental Discovery Rule</p>	<p>Noted. Will comply Council records indicate that there are no known heritage sites within the property. It is not anticipated that any archaeological sites/sensitive material will be found. In the unlikely event that any sensitive material is found, the accidental discovery rule will be implemented.</p>
<p>EW-S4</p>	<p>Site Reinstatement <i>As soon as practicable, but no later than six months from the commencement of works:</i></p> <ol style="list-style-type: none"> <i>1. the earthworks area shall be established, filled and/or recontoured in a manner consistent with the surrounding land; and</i> <i>2. replanted with vegetation, which is the same as, or of similar species, to that which existed on the site prior to the earthworks taking place (if any), except that where the site was vegetation with any plant pest, the site may be replanted with indigenous vegetation from locally sourced genetic stocks or sealed, paved, metaled or built over.</i> 	<p>Noted. Will comply.</p>
<p>EW-S5 <i>*Immediate legal effect.</i></p>	<p>Erosion and Sediment Control <i>Earthworks</i></p> <ol style="list-style-type: none"> <i>1. must for their duration be controlled in accordance with the Erosion and Sediment Control Guidelines for Land Disturbing Activities in the Auckland Region 2016 (Auckland Council Guideline Document GD2016/005); and</i> <i>2. shall be implemented to prevent silt or sediment from entering water bodies, coastal marine area, any stormwater system, overland flow paths, or roads.</i> 	<p>Appropriate erosion and sediment controls will be implemented and maintained in compliance with this standard. Sheet A06 of the Application Plans contained in Attachment 2 shows the erosion and sediment controls that will be implemented and maintained during the proposed earthworks.</p>
<p>EW-S6</p>	<p>Setback <i>Earthworks must be setback by the following minimum distances:</i></p> <ol style="list-style-type: none"> <i>1. earthworks that are:</i> 	<p>The application plans show the extent of the proposed earthworks within the site. It is noted that minimal earthworks are required – a scrape of the topsoil to form the parking areas and the cabins will be placed on pile</p>

	<p>a. supported by engineered retaining walls - 1.5m from a site boundary; or</p> <p>b. not supported by engineered retaining walls - 3m from a site boundary; and</p> <p>2. earthworks must be setback by a minimum distance of 10m from coastal marine area.</p> <p><i>Note: setbacks from waterbodies is managed by the Natural Character chapter.</i></p>	foundations and utilise the terracing within 30 and 32 Selwyn Road to minimise land disturbance.
EW-S7	<p>Land Stability Earthworks must not result in any instability of land at or beyond the boundary of the property where the earthworks occurs.</p>	Will comply.
EW-S8	<p>Nature of Filling Material The fill material shall not:</p> <ol style="list-style-type: none"> 1. contain putrescible, pollutant, inflammable or hazardous components; or 2. consist of material other than soil, rock, stone, aggregate, gravel, sand, silt, or demolition material; or 3. comprise more than 5% vegetation (by volume) of any load. 	Will comply.

DEVELOPMENT PROPOSAL STORMWATER MANAGEMENT REPORT

**30, 32, 34 Selwyn Road
Paihia**

PAIHIA TERRACES LTD
December 2024 | V1



DOCUMENT CONTROL RECORD

Client: Paihia Terraces Ltd.

Project Location: 30, 32, 34 Selwyn Road, Paihia

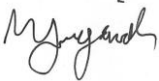
Job Number: J2571-1


Document: Development Proposal | Stormwater Management Report


Version: V1

Document Status: Draft Only

Date: December 2024

Prepared by: 
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Reviewed by: 
Tony Smith, Technical Director

Approved by: 
Tony Smith, Technical Director

Filename: <https://gweconsult.sharepoint.com/sites/ActiveProjects/COM/30, 32, 34 Selwyn road, Paihia-J2571-1/05-SW/04-Reports>

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1 INTRODUCTION

This report provides a summary of the stormwater management design for the proposed development at 30, 32, 34 Selwyn Road, Paihia. This report was prepared by GWE Consulting Ltd (GWE) for our client Paihia Terraces Ltd. in accordance with our fee proposal dated 05 November 2024.

The purpose of this report is to assess the hydrological conditions of the site and to provide a suitable design for the discharge of stormwater from the proposed development. The report may be used for submission to Far North District Council in support of Resource and/or Building Consents.

2 SITE DESCRIPTION

The subject site consists of the following three properties:

- 30 Selwyn Road, Paihia - Lot 3 DP 328135 - 829 m²
- 32 Selwyn Road, Paihia - Lot 2 DP 328135 - 691 m²
- 34 Selwyn Road, Paihia - Lot 1 DP 328135 - 760 m²

The site is located approximately 140 m from the shoreline and is classified as commercial zone within the Far North District Council Operative Plan. Currently, there are two houses and a shop located on the site. The terrain is gently sloping towards the east. Refer to Figure 1 for the topographic survey plan and Figure 2 for the site location

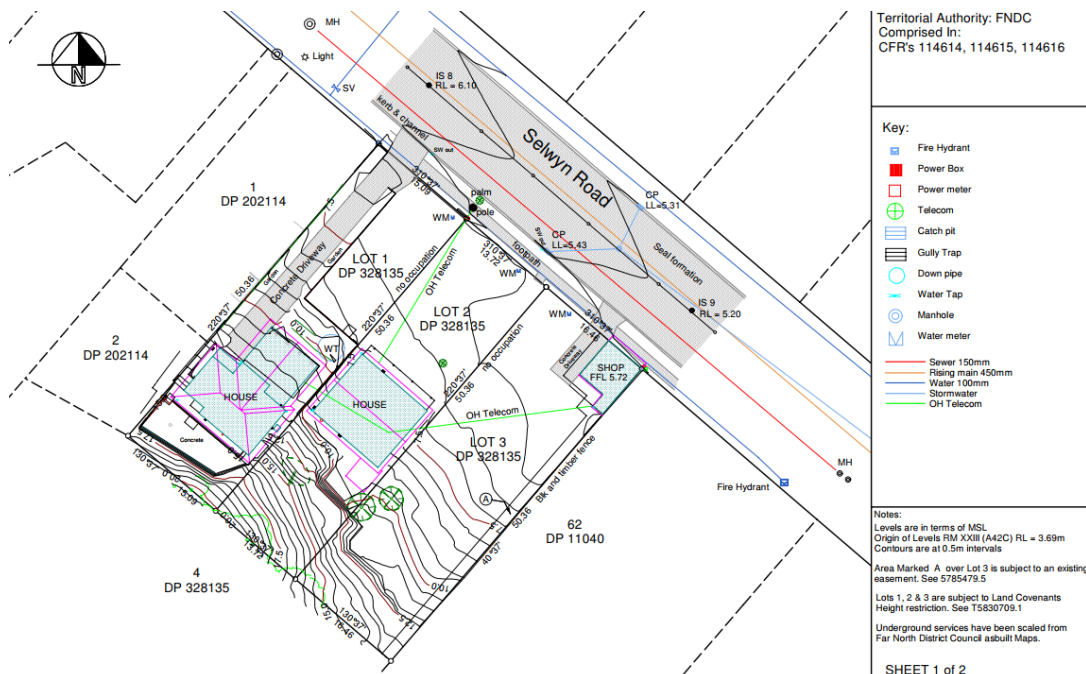


Figure 1: Topographic Survey Plan

Source: Topographic Survey Plan by Donaldsons, dated 12 November 2014, Job No. 6537



Figure 2: Site Location Plan (site location indicated in blue)

Source: <https://fndc.maps.arcgis.com/apps/webappviewer/index.html?id=06922e6ff50e45bc98aef82dc539fc53>

3 PROPOSED DEVELOPMENT

GWE has been provided with architectural drawings of the proposed development by CTM Architectural Ltd dated 5th November 2024. Based on the drawings, GWE understands that the house on Lot 2 is to be demolished and removed, and 5 units of single-storey dwellings and a 20-lot carpark are proposed across the three lots.

The existing house and shop on lot 1 and 3 respectively, are to be maintained. Cut and fill will be done in appropriate areas to provide more level building platforms and carparking areas. A new vehicle crossing will be provided from Selwyn Road at the eastern property boundary. The proposed site plan is shown in Figure 3 below.

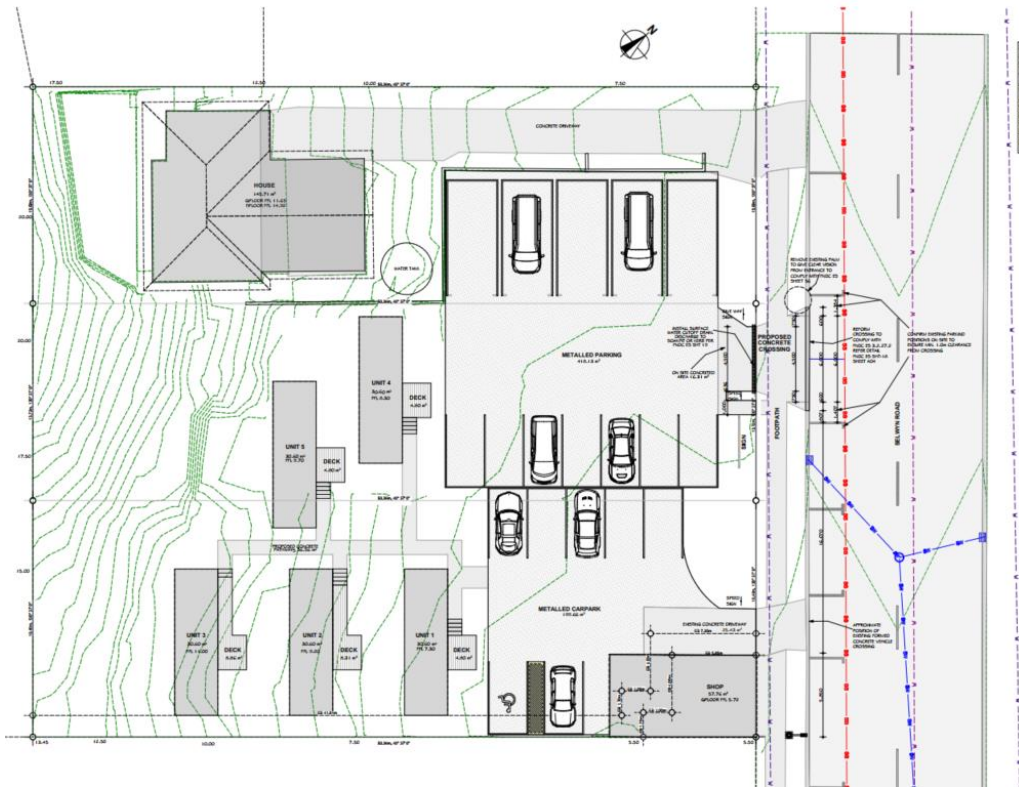


Figure 3: Proposed Site Plan

Source: Proposed Site Plan by CTM Architecture Ltd (dated November 2024)

Table 1: Impervious Area Breakdown

CATEGORY	PRE-DEVELOPMENT	POST-DEVELOPMENT	DIFFERENCE
Roof Areas (m ²)	314.64	356.47	+ 41.83
Concreted Areas (m ²)	135.73	780.96	+ 645.23
Total Impervious Area (m ²)	450.37	1137.43	+ 687.06
Total Site Area (m ²)	2280	2280	-
Percentage Impervious	19.75%	49.89%	+ 30.14

Notes:

- Existing and proposed areas are based on architectural drawings by CTM Architecture Ltd (dated November 2024)

4 REGULATORY AND DESIGN REQUIREMENTS

4.1 Design Criteria

ITEM	CRITERIA	
CN / C	Pervious	74 / 0.59
	Impervious	98 / 0.96
Rainfall Depths	10yr, 24hr ARI	107 mm (NIWA HIRDS)
	100yr, 24hr ARI	165 mm (NIWA HIRDS)

ITEM	CRITERIA	
Water Quality	Buildings	<ul style="list-style-type: none"> No high contaminant yielding roofing or cladding. Leaf guards/first flush devices on guttering and downpipes
	Carparks	<ul style="list-style-type: none"> High contaminant generating activity. Runoff to pass through propriety devices i.e., StormFilters
	Other Impervious areas	<ul style="list-style-type: none"> No high contaminant generating activity. Collection of flows in catchpits containing EnviroPods prior to discharge
Conveyance	Primary drainage	<ul style="list-style-type: none"> Collect, convey and discharge 10yr ARI flow in sealed pipe network without causing increase in flooding of other properties or downstream scouring/erosion. Minimise uncollected impervious areas and avoid concentrated flow from any uncollected surfaces
	Secondary OLFP	<ul style="list-style-type: none"> OLFPs entry and exits points of the site will not be altered.
Flood Management	2yr ARI / 10yr ARI	<ul style="list-style-type: none"> Site located in directly upstream of flood prone area, buildings downstream are at risk of flooding. On-site flow attenuation for the 2- & 10-year ARI rainfall events up to 80% pre-development levels is required.

5 PROPOSED STORMWATER MANAGEMENT

5.1 Existing Public Stormwater Network

As indicated by Far North Maps, there is a public manhole (GIS ID: 3505) and a DN600 public concrete stormwater pipeline (GIS ID: 3755) located within the Selwyn Road carriageway that runs towards the southeast. The current stormwater connection location and conditions is unknown and is to be confirmed on site. Refer to Figure 4 below for a plan showing the location of the stormwater assets.



Figure 4: Existing Stormwater Network Layout

Source: [Aerial Imagery \(LINZ\)](#)

5.2 Proposed Stormwater Connection

i. Overview

The existing stormwater connection is to be located on site and, if possible, shall be retained for the discharge of stormwater from the proposed addition of 5 small units and a carpark into the public network. We propose a new DN375 connection will be made from the site to the existing stormwater manhole (GIS ID: 3505, see 4 above) on Selwyn Road. All works are to comply with Far North District Council Engineering Standards & Guidelines and the New Zealand Building Code E1: Surface Water.

A catchment analysis was undertaken to compare the pre-development and post-development runoff and assess the potential effects on the public network. The pre-development and post-development peak flows during the 10% AEP 10-minute storm events were calculated based on the impervious and pervious areas extracted from the architecture plans by CTM Architectural Ltd.

The design rainfall intensities were obtained from NIWA's High Intensity Rainfall Data System (HIRDS), with the effects of climate change (2.1°C temperature increase). A summary of the existing and proposed site areas and their associated peak flows is presented in 2 below. The supporting calculations are attached in Appendix A.

Table 2: Summary of Existing and Proposed Impervious Areas

AREAS	PRE-DEVELOPMENT		POST-DEVELOPMENT		Difference (L/s)
	Area (m ²)	Peak Flow (L/s)	Area (m ²)	Peak Flow (L/s)	
Impervious	450.4	15.8	1137	39.9	+24.1

AREAS	PRE-DEVELOPMENT		POST-DEVELOPMENT		
Pervious	1829.6	21.4	1142.6	13.4	-8
Total	2280	37.2	2280	53.3	+16.1

Table 2 shows that after accounting for effects of climate change, an increase in peak flow by 16.1 L/s is expected from the site for a 10% AEP 10-minute storm event post-development.

ii. Network Capacity Assessment

The existing network comprising for this site a DN600 stormwater pipe (GIS ID: 3762, see Figure 10 for pipe location) discharges to the Paihia waterfront as shown below. The catchment is approximately 23,834 m² which consists of approximately 14,252 m² impervious area and 9,582 m² pervious area. Refer to Figure 6 for a sketch of the catchment.



Figure 5: Downstream Stormwater Pipe Location

Source: Far North Maps - <http://apps.geocirrus.com/FNDC/index.html?viewer=farnorthmaps>

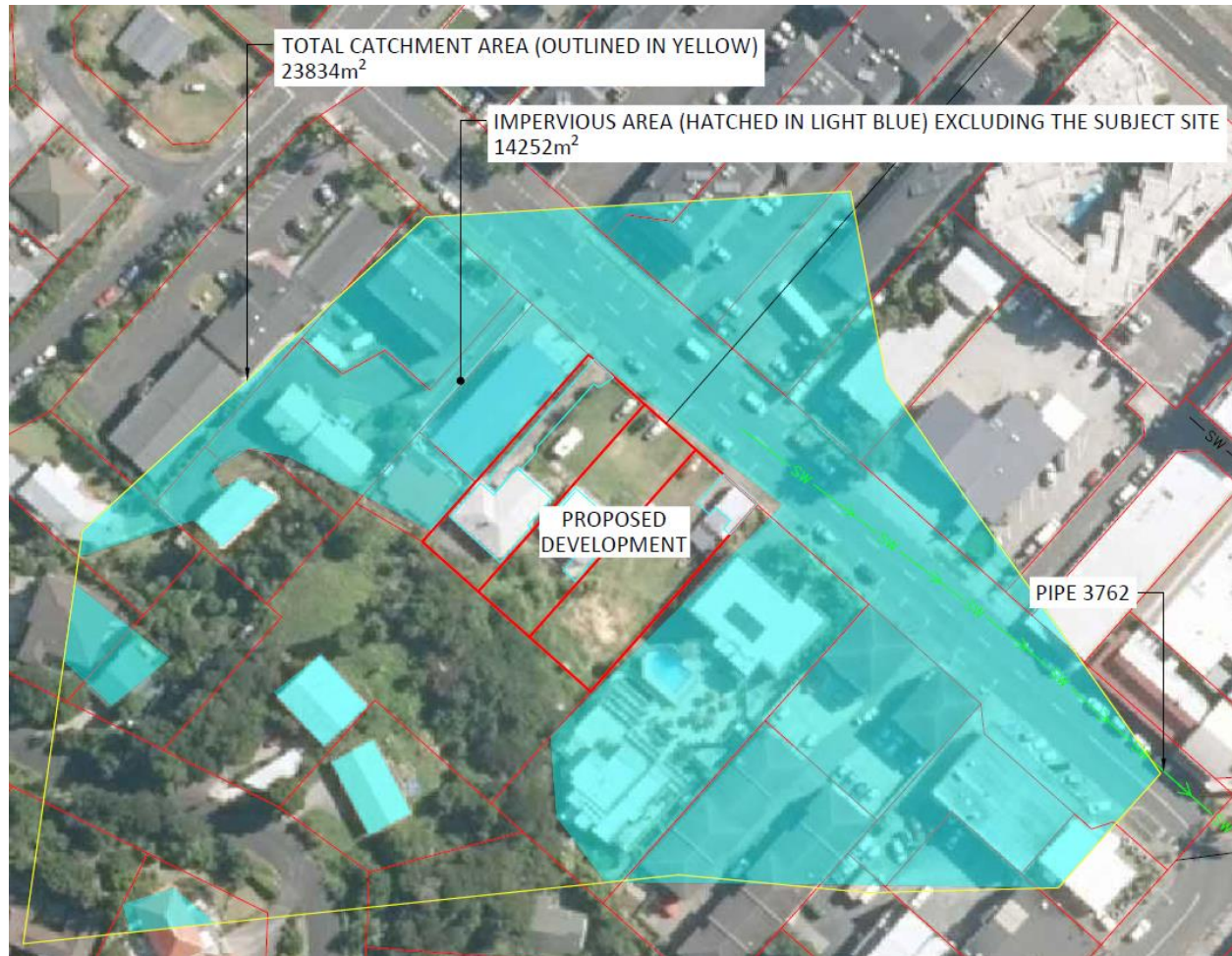


Figure 6: Pipe 3762 Catchment (based on Far North Maps contours)

Source: *Far North Maps* - <http://apps.geocirrus.com/FNDC/index.html?viewer=farnorthmaps>

A pipe capacity analysis was carried out to assess the ability of the DN600 public stormwater pipe to cater for the total post-development peak flow of 666 L/s from the catchment during a 10% AEP 10-minute event, including the effects of climate change (3.8°C temperature increase). A survey conducted by Donaldsons identified the invert levels of the pipe. 3 below summarises the pipe capacity assessment. Refer to Appendix A for the supporting calculations.

Table 3: Summary of Stormwater Pipeline Capacity Assessment

PIPE	GRADE (%)	CAPACITY (L/s)	10-YR ARI PEAK FLOW (L/s)	RESIDUAL PIPE CAPACITY (L/s)
DN600, GIS ID: 3762 Note1	1.25 Note 2	685	666 Note 3	19

Note:

1. Refer to Figure 10 for pipe location.
2. 1.25% is the grade calculated based on the invert levels identified by the topographic survey.
3. This is the total anticipated post-development peak flow rate from the upstream catchment, including the development site, which will be serviced by this pipe.

The capacity assessment indicates that the existing DN600 stormwater pipe has adequate capacity to cater for the post-development runoff from the proposed development. As the post development peak flow through the pipe (666 L/s) is less than the capacity of the pipe (685 L/s), the proposed development is expected to have a less than minor effect on the existing network.

iii. Runoff Water Quality

The proposed car park on the ground floor has been mitigated using filtration based on the relevant standards.

6 CONCLUSIONS AND RECOMMENDATIONS

GWE Consulting has been engaged to provide a Stormwater Management Report for the proposed development at 30, 32, 34 Selwyn Road, Paihia. The proposed development will be serviced as per the following:

The existing public stormwater connection shall be replaced by a new DN375 pipe to serve the dwellings and the carparks. The pipe capacity assessment confirms that there is adequate capacity within the network to accommodate the proposed development.

Detailed design is to be completed during building consent where pipe and manhole sizes will be confirmed. All works are to comply with Far North District Council Engineering Standards & Guidelines and the New Zealand Building Code E1: Surface Water.

7 LIMITATIONS

This report has been prepared for the sole benefit of **Paihia Terraces Ltd.** as our Client, and their appointed representatives, according to their instructions, for the specific objectives described herein. This report is qualified in its entirety and should be considered in the light of our Terms of Engagement with the Client and the following:

1. Data or opinions contained within the report may not be used in other contexts or for any other purpose without our prior review and written agreement. Any reliance will be at the parties' sole risk.
2. No responsibility is assumed for inaccuracies in reporting by the information providers. In no event, regardless of whether GWE 's consent has been provided, does GWE accept any liability, whether directly or indirectly, for any liability or loss suffered or incurred by any third party to whom this report is disclosed placing any reliance on this report, in part or in full.
3. GWE has relied on information provided by the Client and by third parties to produce this document and arrive at its conclusions. GWE has not verified information provided (unless specifically noted otherwise) and we assume no responsibility and make no representations with respect to the adequacy, accuracy, or completeness of such information.

APPENDIX A
ARCHITECTURAL DRAWINGS



PROJECT
DEVELOPMENT STAGE 1

FOR
PAIHIA TERRACES Ltd.

SITE ADDRESS
**30-34 SELWYN ROAD,
 PAIHIA
 BAY OF ISLANDS
 FAR NORTH DISTRICT COUNCIL**

ISSUE
SCHEME 4 - PRELIM RC

DATE
05 NOV 2024

PROJECT REF
24-059



Layout	Rev	Sheet Name
Plans		
A01		COVER SHEET
A02		EXISTING SITE PLAN
A03		PROPOSED SITE PLAN
A04		PARKING
A05		SITE OVERVIEW
A06		ELEVATIONS
A07		ELEVATIONS
A08		VISUALS 1
A09		VISUALS 2
A10		VISUALS 3
A11		WHITE MODEL



APPENDIX 3D: MANOEUVRING & PARKING SPACE DIMENSIONS

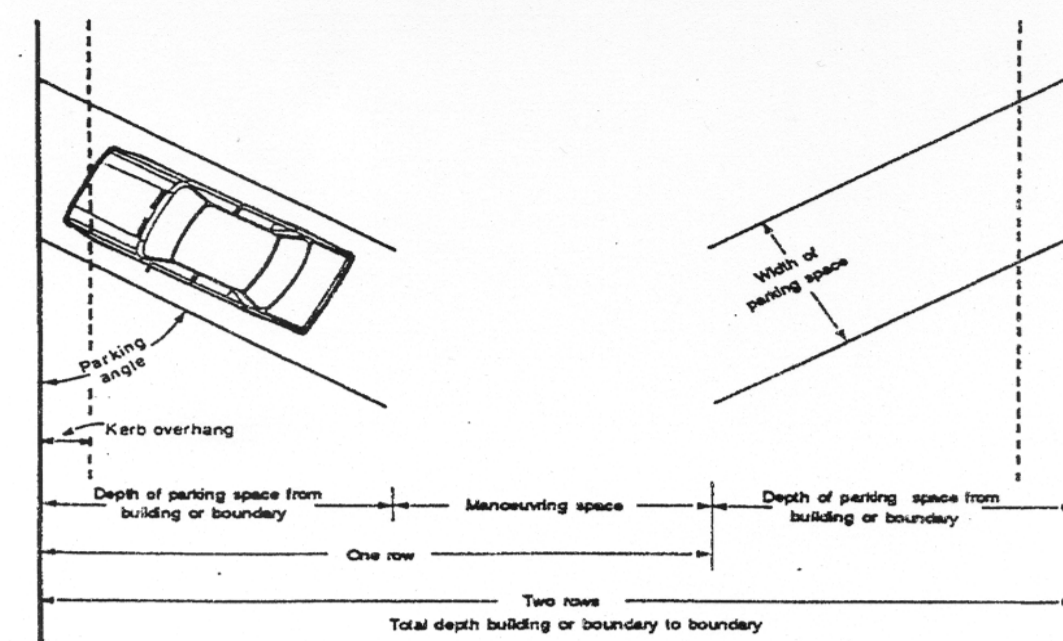
(Reference: Part 3 District Wide Provisions, Section 15.1 Traffic, Parking and Access)
(all dimensions in metres)

Parking Angle	Width of Parking Space	Kerb Overhang	Depth of Parking Space	Manoeuvring Spaces	Total Depth One Row	Total Depth Two Rows
90° Regular Users ⁽¹⁾	2.4 ⁽²⁾	1.0	4.9	7.1	12.9	16.9
	2.5	1.0	4.9	6.7	11.6	16.5
	2.6	1.0	4.9	6.3	11.2	16.1
90° Casual Users ⁽¹⁾	2.7	1.0	4.9	5.9	10.8	15.7
	2.75	1.0	4.9	5.9	10.8	15.7
	2.7	1.0	4.9	6.7	11.6	16.5
75°	2.4 ⁽²⁾	1.0	5.2	6.6	11.6	16.4
	2.5	1.0	5.2	6.0	11.2	16.4
	2.6	1.0	5.2	5.7	10.9	16.1
60°	2.7	1.0	5.2	5.0	10.2	15.4
	2.75	1.0	5.2	4.3	9.5	14.7
	2.4 ⁽²⁾	1.0	5.2	4.6	9.8	15.0
45°	2.5	1.0	5.2	4.1	9.3	14.5
	2.6	1.0	5.2	3.5	8.7	13.9
	2.7	1.0	5.2	3.3	8.5	13.7
30°	2.7	0.8	4.9	3.2	8.4	13.6
	2.75	0.8	4.9	2.9	7.8	12.7
	2.4 ⁽²⁾	0.8	4.9	2.7	7.6	12.5
Parallel	2.6	0.8	4.9	2.5	7.4	12.3
	2.7	0.8	4.9	2.4	7.3	12.2
	2.7	0.8	4.9	2.3	7.2	12.1
Parallel	5.9	0.4	2.5	3.6	6.1	8.6
	6.1	0.4	2.5	3.3	5.8	8.3
	6.3	0.4	2.5	3.0	5.5	8.0

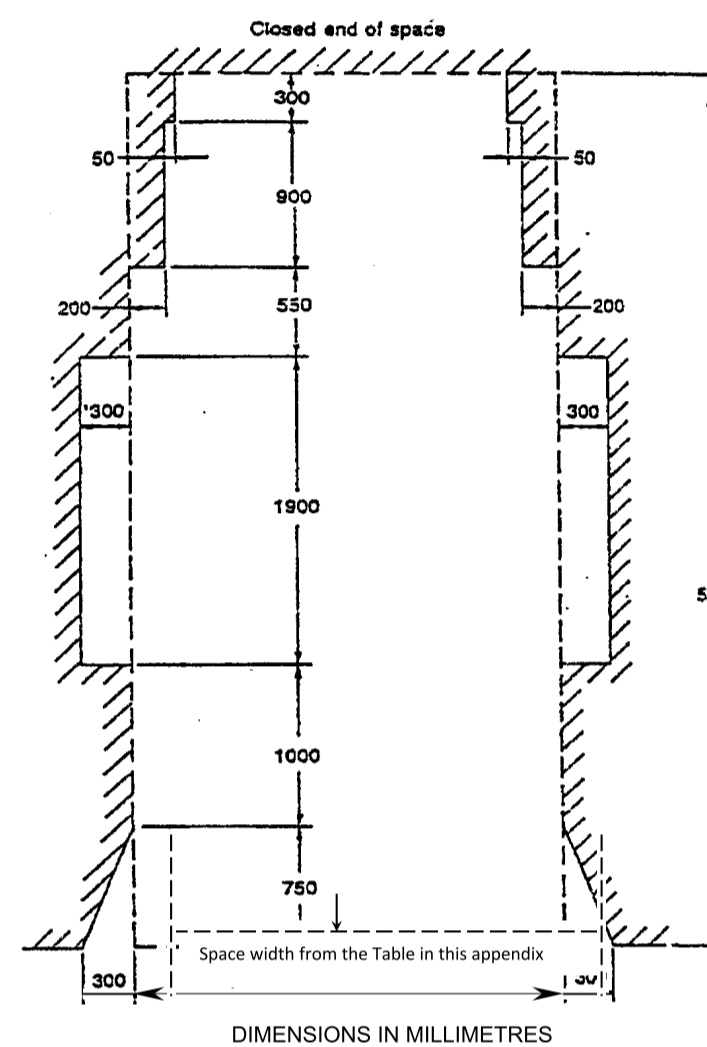
- Regular users are people whose regular use gives them a familiarity with the building that permits smaller safe clearances between vehicles and parts of buildings. Casual users are people (usually short-term visitors) who would not be familiar with the building layout.
 - Stall widths of 2.4m should generally only be used where users are familiar with the car park. This stall width does not meet the requirements of the Building Code for Casual Users.
- Notes:**
- Minimum aisle widths are 3.6m for a one-way aisle, and 5.5m for a two-way aisle. Where an aisle serves more than 50 spaces, it should be designed as a circulation route in which case the minimum width for a two-way aisle increases to 6.5m. Note that the Building Code requires an extra 0.8m width where pedestrians use a vehicle circulation route.
 - Stall widths shall be increased by 0.3m where they abut obstructions such as columns or walls.
 - All overhang areas shall be kept clear of objects greater than 150mm in height.
 - Where parallel end spaces have direct access through the end of the stall the length of the stall may be reduced to 5.4m.
 - One-way traffic is assumed for angle spaces.
 - Car park height shall be at least 2.3m over the full area of the space, except where special provision is made to divert over height vehicles, in which case the minimum height may be reduced to 2.1m.
 - Note that the Building Code may require car park spaces to be provided for people with disabilities. Details of the requirements for these spaces may be found in NZS 4121.
 - Linear interpolation is permitted for stall width, parking angle and aisle width.
 - Car park spaces which comply with the preferred design envelope shown below are deemed to comply with the dimensions in this Appendix.

Opp Plan - Appendix-3-Traffic-and-parking

Manoeuvring and Parking Space Dimensions:

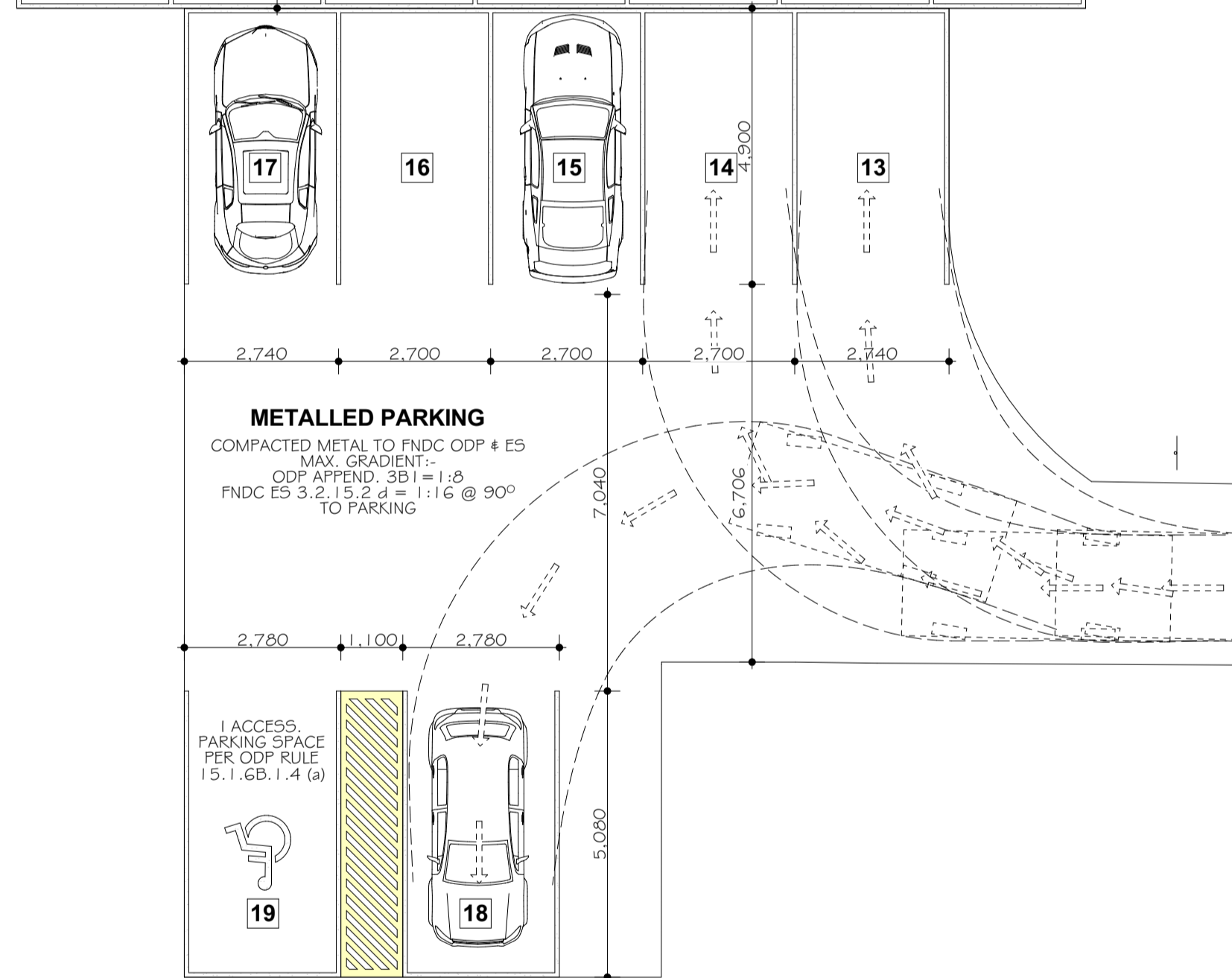
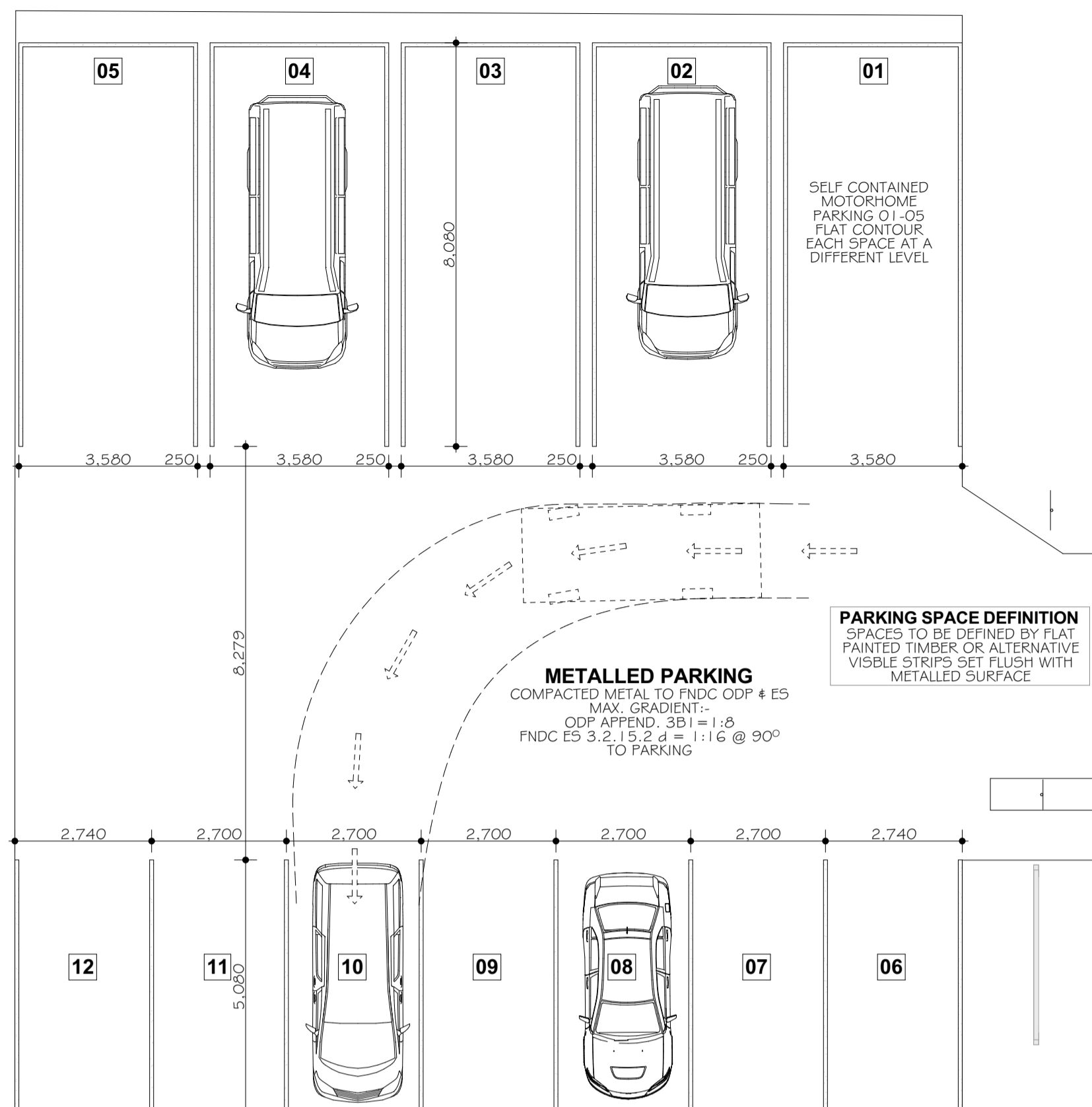


Preferred Design Envelope around Parked Vehicle to be kept clear of Columns, Walls and Obstructions:



Note: The preferred design envelope provides for structural elements to be clear of all four side doors whereas the standard provides for the opening of the front door only (when nose in).

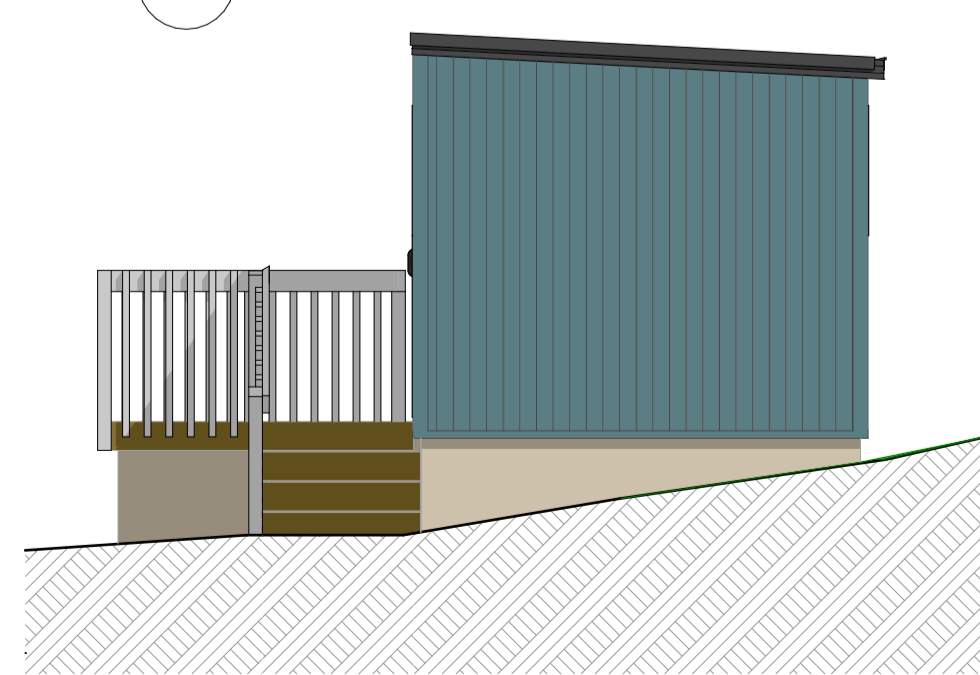
Opp Plan - Appendix-3-Traffic-and-parking



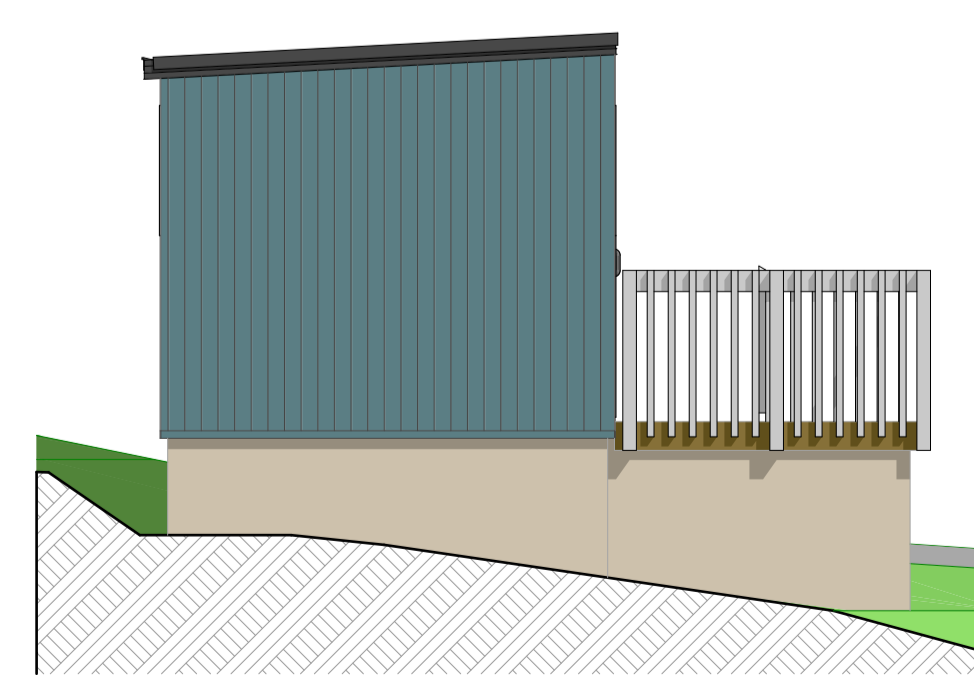
PARKING & MANEUVERING
1:100



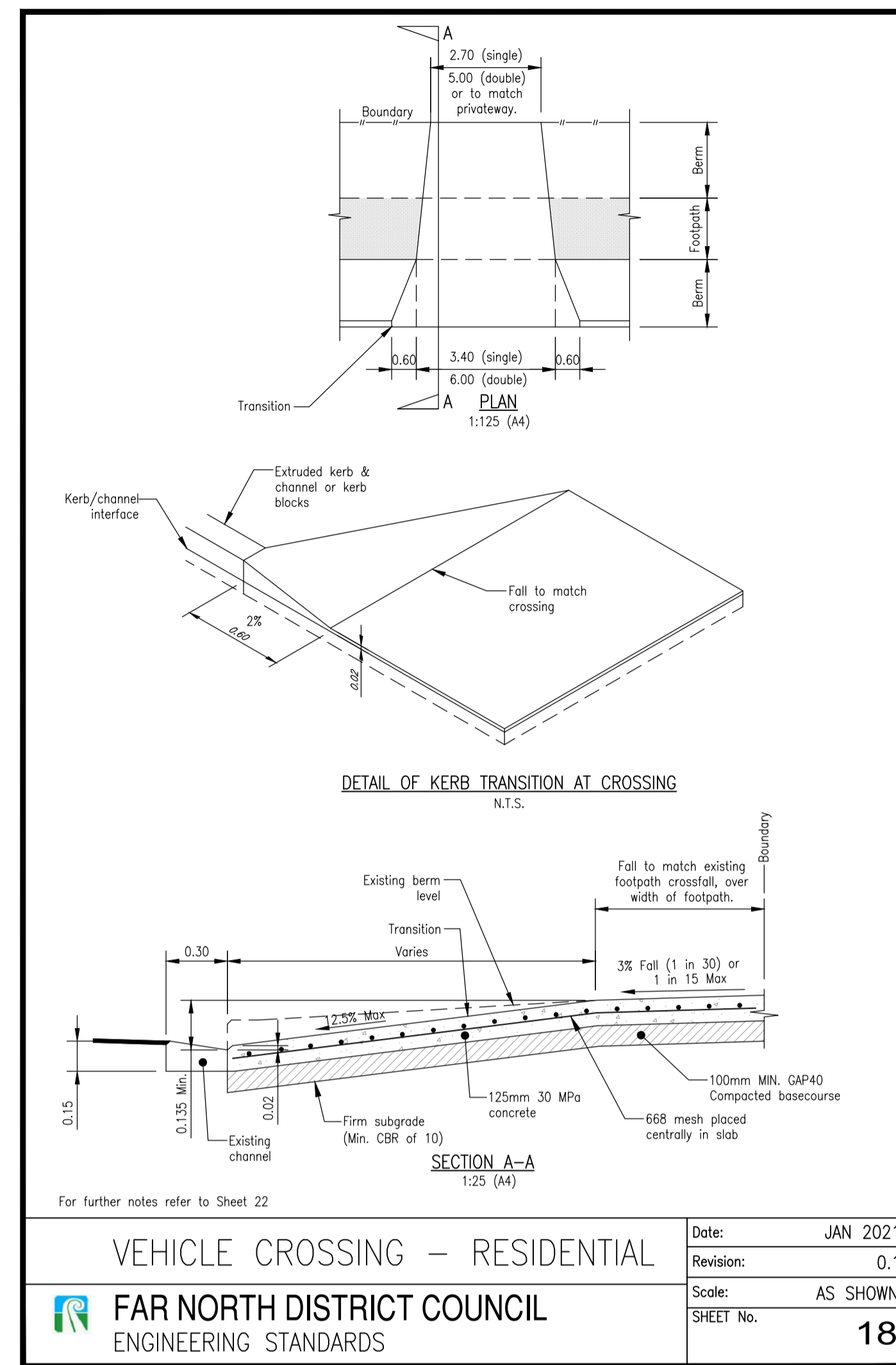
UNIT 1 NORTH EAST ELEVATION
1:50



UNIT 1 NORTH WEST ELEVATION
1:50



UNIT 1 SOUTH EAST ELEVATION
1:50



VEHICLE CROSSING – RESIDENTIAL

FAR NORTH DISTRICT COUNCIL
ENGINEERING STANDARDS

Date: JAN 2021
Revision: 0.1
Scale: AS SHOWN
SHEET No. 18

CTM ARCHITECTURAL LTD

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PHONE: 09 238 4414

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WORK TO N.Z.B.C. AND RELEVANT CODES
DIMENSIONS SUPERSEDE SCALE
CHECK AND VERIFY ALL LEVELS AND DIMENSIONS ON SITE BEFORE STARTING ANY WORK.

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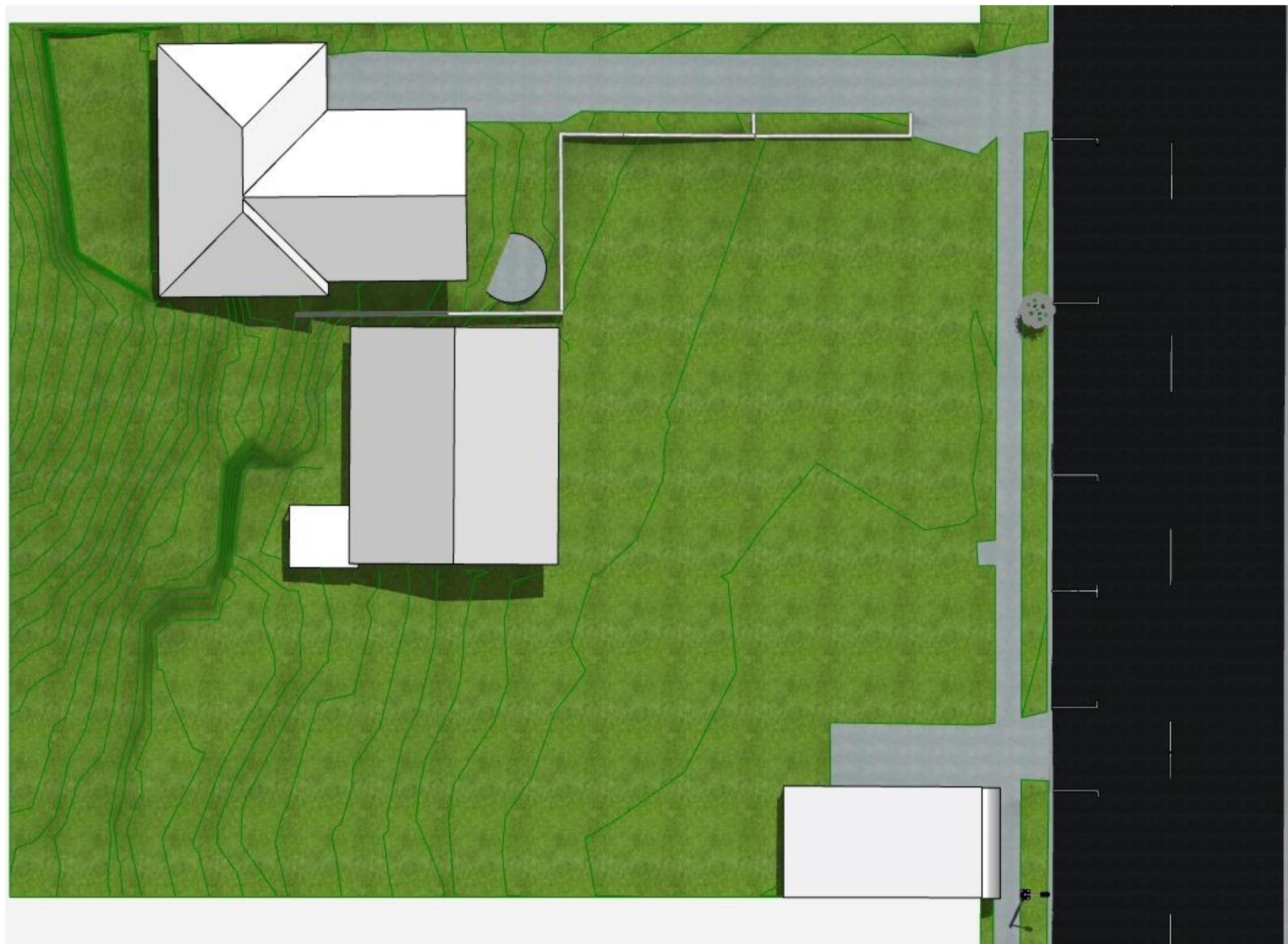
PROJECT STATUS
SCHEME 4 - PRELIM RC

DRAWN: CRAIG
DATE: 05 NOV 2024
JOB REF: 24-059

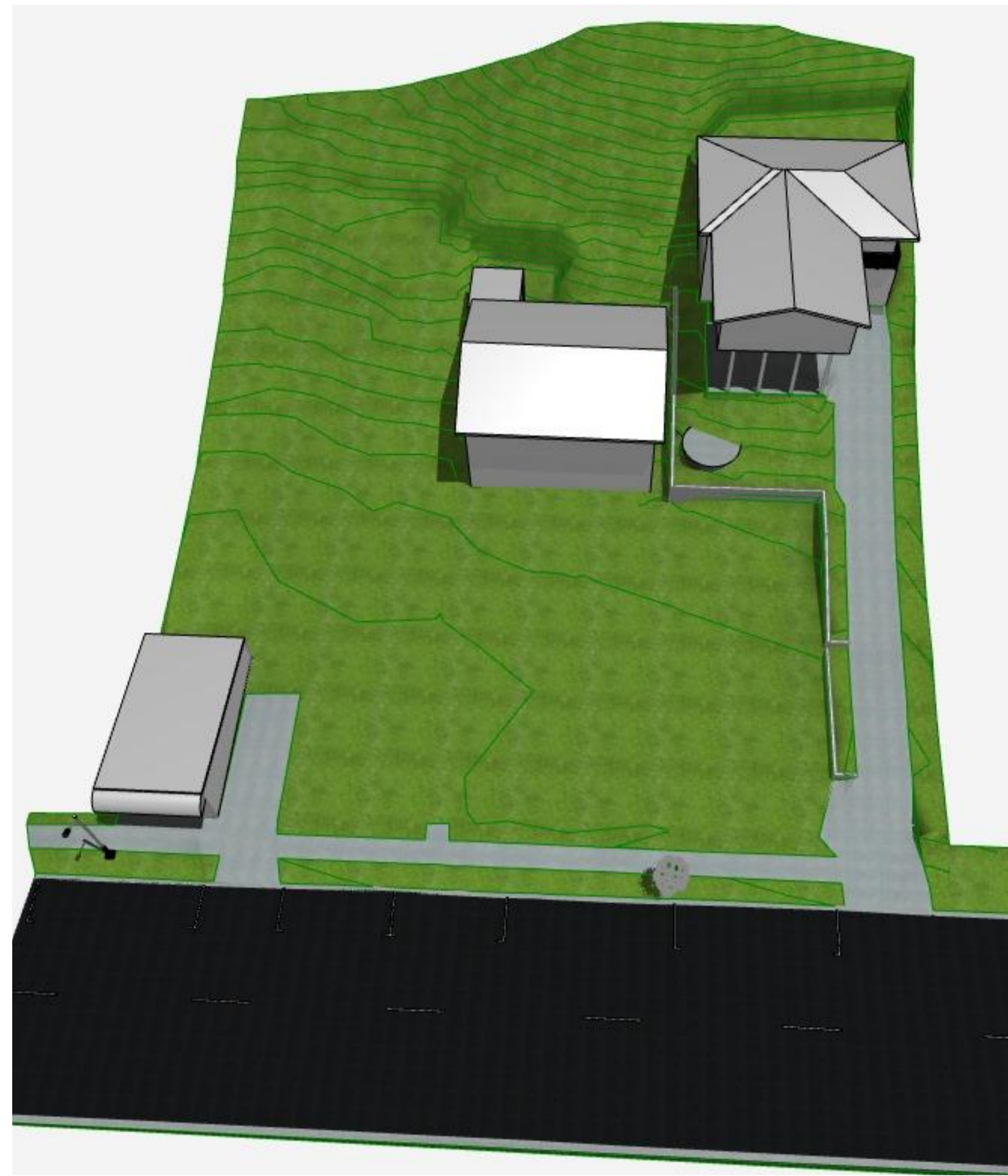
SCALE: AS SHOWN
SHEET FORMAT: A1
TOTAL SHEETS: 11

A04

SHEET REVISION



EXISTING



EXISTING



PROPOSED



PROPOSED

RevID	CHD	Revision	DATE
CLIENT			
PAIHIA TERRACES Ltd.			
ADDRESS			
30-34 SELWYN ROAD, PAIHIA BAY OF ISLANDS FAR NORTH DISTRICT COUNCIL			
PROJECT			
DEVELOPMENT STAGE 1			



CTM ARCHITECTURAL LTD
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web: www.ctm.net.nz
e-mail: info@ctm.net.nz

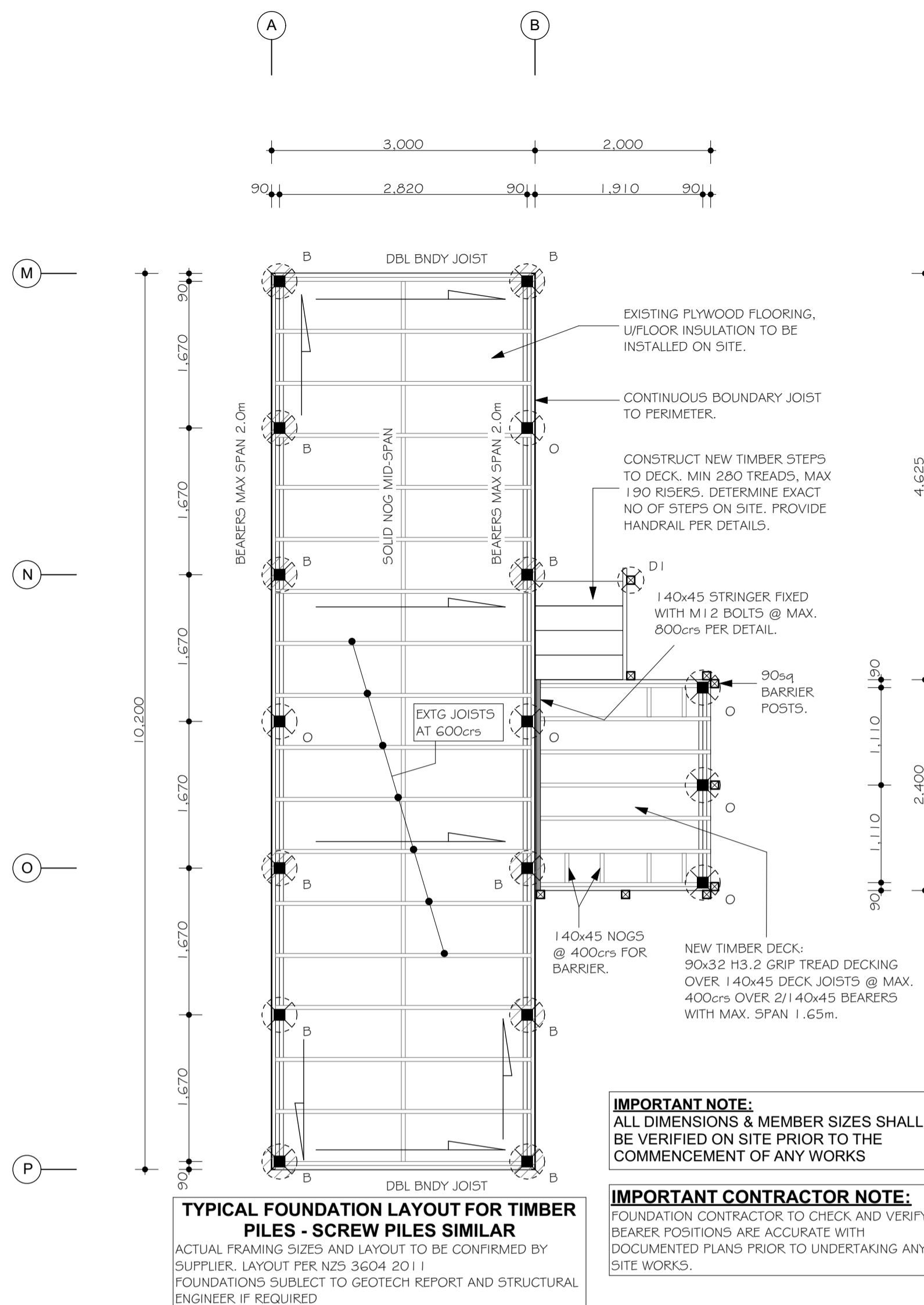
WORK TO N.Z.B.C. AND RELEVANT CODES
DIMENSIONS SUPERSEDE SCALE
CHECK AND VERIFY ALL LEVELS AND DIMENSIONS
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ISSUE
SCHEME 4 - PRELIM RC

DRAWING NAME
SITE OVERVIEW

DRAWN	SCALE	A05
CRAIG	AS SHOWN	
DATE	SHEET FORMAT	SHEET
05 NOV 2024	A1	
JOB REF	TOTAL SHEETS	REVISION
24-059	11	



TIMBER SUB-FLOOR KEY:

ALL DEPTHS ARE BELOW CLEARED GROUND LEVEL (C.G.L.) AND THIS SHALL BE ALLOWED FOR IF THE PLATFORM IS NOT CLEARED OF TOPSOIL ETC PRIOR TO FOUNDATION WORKS.

MIN STRENGTH FOR CONC FOOTINGS = 17.5MPa

PILES & CONC FOOTING SIZES AS PER BELOW:

O DENOTES ORDINARY SQUARE PILES
USE 125x125sq TIMBER PILE EMBEDDED 500mm (FOR MIN 100mm BASE COVER) INTO MIN. 400Ø x 600mm DEEP CONC. PILE FOOTING, SET MIN. 600 BELOW C.G.L.
ORDINARY PILE FIXINGS AT TOP.

B DENOTES BRACED SQUARE PILES
USE 125x125sq TIMBER PILE EMBEDDED 500mm (FOR MIN 100mm BASE COVER) INTO MIN. 400Ø x 600mm DEEP CONC. PILE FOOTING, SET MIN. 600 BELOW C.G.L.
1.2ØN PILE FIXINGS AT TOP.

D1 DENOTES ORDINARY DECK BARRIER POST IN FOOTING
USE 90x90sq TIMBER POSTS EMBEDDED 500mm (FOR MIN 100mm BASE COVER) INTO MIN. 400Ø x 600mm DEEP CONC. PILE FOOTING.
ORDINARY PILE FIXINGS AT TOP.

SUBSTITUTE ANCHOR PILES WITH BRACED PILE SYSTEM (IN BOTH DIRECTIONS) WHERE PILE HEIGHT EXCEEDS MAX 600mm.

DENOTES SUBFLOOR BRACE, SIZE AS FOLLOWS (ARROW POINTS TO LOWER END)
NOT EXCEEDING 3.0m - 100x75
NOT EXCEEDING 5.0m - 100x100

PILE NOTE:
PILES TO BE MINIMUM 150mm HIGH ABOVE FINISHED GROUND LEVEL. INSTALL DPC BENEATH BEARERS WHERE PILES ARE LESS THAN 300mm HIGH.

SUBFLOOR VENTING
SUBFLOOR VENTILATION TO BE PROVIDED BY TIMBER BASE BOARDS WITH MINIMUM 20mm GAPS GIVING REQUIRED VENTILATION AS PER NZS 3604.

SUBFLOOR NOTES:
GROUND LEVEL VARIES. PROVIDE MIN 450mm CRAWL SPACE TO USIDE OF HOUSE JOISTS.
INSTALL BASE BOARDS WITH MIN 20mm VENTING GAPS TO SUB FLOOR FRAME.
PROVIDE SUBFLOOR ACCESS HATCH.
HOUSE BEARERS MAX. SPAN 2.0m. ENSURE SOLID BLOCKING AT 1800c/s MAX OVER ALL BEARER SUPPORTS.
C.O.S POSITION OF ANY BEARER JOINS & ENSURE A PILE IS LOCATED DIRECTLY BENEATH. ADJUST PILES TO SUIT IF REQUIRED (MAX. BEARER SPAN AS NOTED).
ENSURE MIN. 12mm GAP BETWEEN NEW DECKS & CLADDING

IMPORTANT NOTE:
ALL DIMENSIONS & MEMBER SIZES SHALL BE VERIFIED ON SITE PRIOR TO THE COMMENCEMENT OF ANY WORKS

IMPORTANT CONTRACTOR NOTE:
FOUNDATION CONTRACTOR TO CHECK AND VERIFY BEARER POSITIONS ARE ACCURATE WITH DOCUMENTED PLANS PRIOR TO UNDERTAKING ANY SITE WORKS.

TYPICAL FOUNDATION PLAN - UNIT 1
1:50

GIB EzyBrace® Bracing Software

Demand Calculation Sheet

Job Details

Name: Proposed Relocation of Dwelling
Street and Number: 30-34 Selwyn Street, Pahiia
Lot and DP Number: Lot 1, 2 & 3 DP 328135
City/Town/District: Far North Council
Designer: Sheryl
Company: CTM Architectural Ltd
Date: Friday, 1 November 2024

Building Specification

Number of Storeys: 1
Floor Loading: 2 kPa
Foundation Type: Subfloor
Subfloor Cladding Weight: Light
Cladding Weight: Single
Roof Weight: Light
Room in Roof Space: No
Roof Pitch (degrees): 3
Roof Height above Eaves (m): 0.3
Building Height to Apex (m): 3.5
Ground to Lower Floor (m): 1.0
Average Stud Height (m): 2.4
Building Length (m): 10.2
Building Width (m): 3.0
Building Plan Area (m²): 30.60

Building Location

Wind Zone = High
Earthquake Zone 1
Soil Type: D & E (Deep to Very Soft)
Annual Prob. of Exceedance: 1 in 500 (NZS3604:2011 Default)

Bracing Units required for Wind		Bracing Units required for Earthquake	
	Along	Across	Along & Across
Single Level	84	255	140
Subfloor Level	386	614	205

GIB EzyBrace® Bracing Software

Subfloor Level Along Resistance Sheet

Job Name: Proposed Relocation of Dwelling

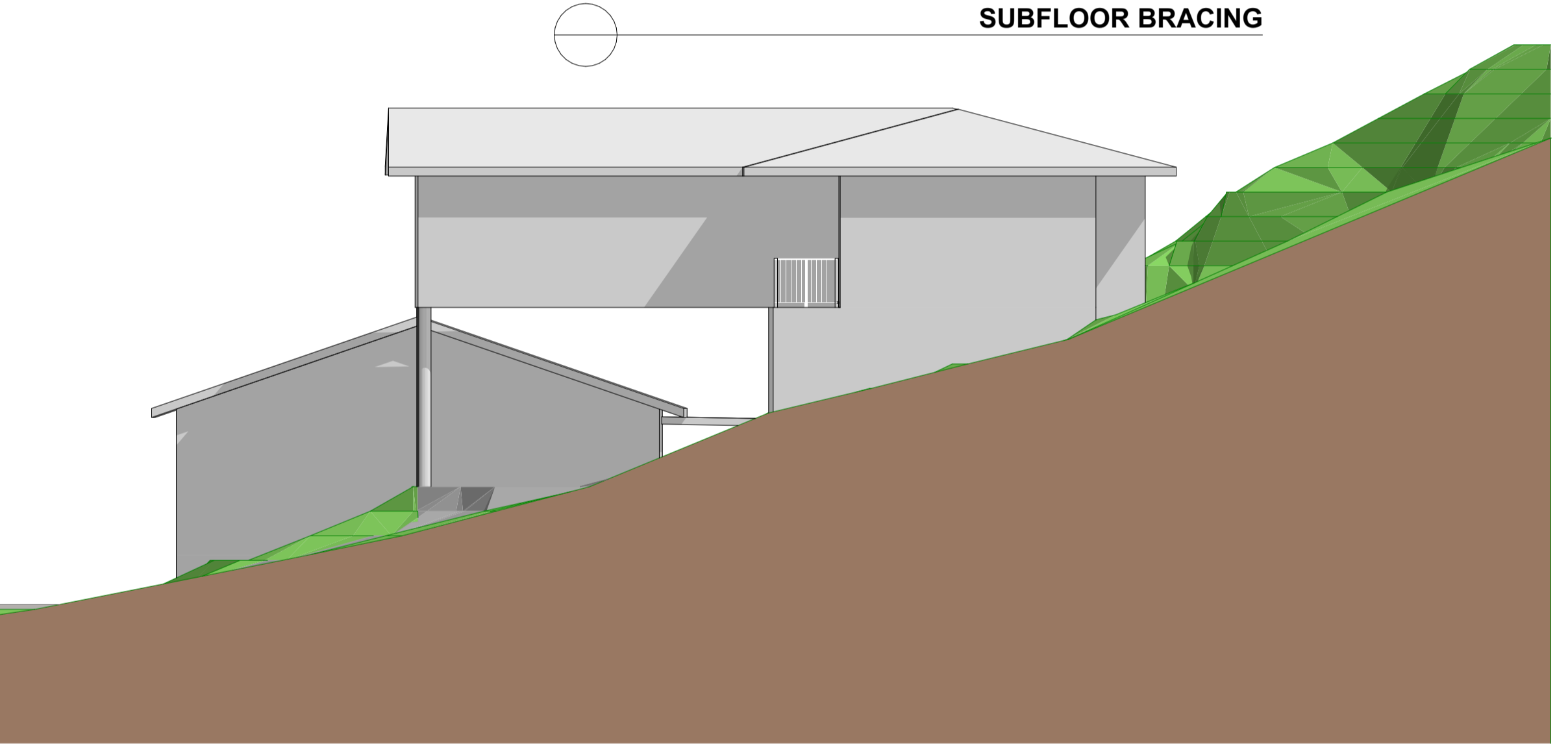
Line	Element	Length (m)	Angle (degrees)	Type	Supplier	W60 (kg)	E0 (kg)	Demand		Achieved	
								W60	E0	W60	E0
A	1	2.00		Braced Pile	NZS3604	320	240	124%	176%	180 OK	236 OK
B	1	1.00		Braced Pile	NZS3604	160	120	100 OK	120 OK	160 OK	120 OK

GIB EzyBrace® Bracing Software

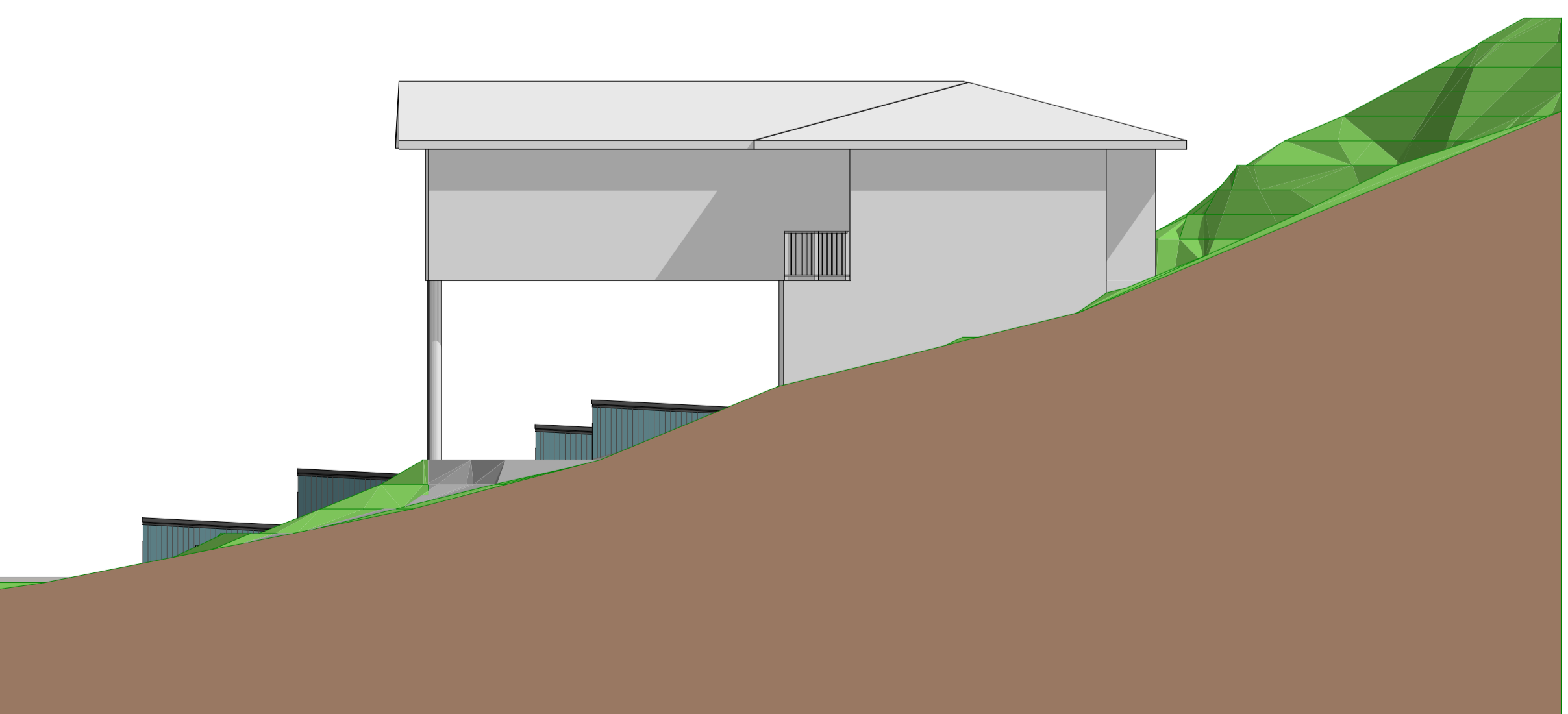
Subfloor Level Across Resistance Sheet

Job Name: Proposed Relocation of Dwelling

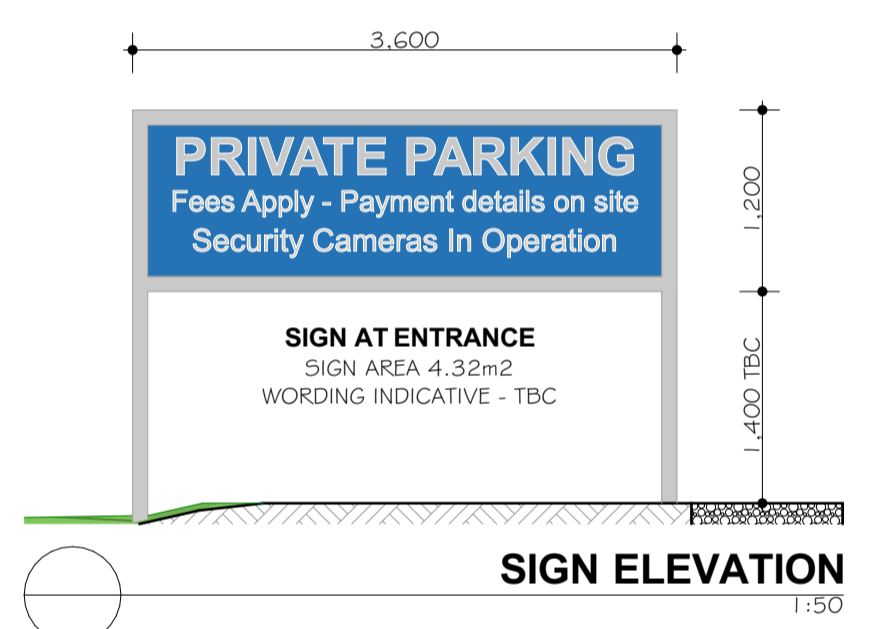
Line	Element	Length (m)	Angle (degrees)	Type	Supplier	W60 (kg)	E0 (kg)	Demand		Achieved	
								W60	E0	W60	E0
M	1	1.00		Braced Pile	NZS3604	160	120	100 OK	120 OK	160 OK	120 OK
N	1	1.00		Braced Pile	NZS3604	160	120	100 OK	120 OK	160 OK	120 OK
O	1	1.00		Braced Pile	NZS3604	160	120	100 OK	120 OK	160 OK	120 OK
P	1	1.00		Braced Pile	NZS3604	160	120	100 OK	120 OK	160 OK	120 OK



EXISTING NORTH WEST ELEVATION
1:100



PROPOSED NORTH WEST ELEVATION
1:100



SIGN ELEVATION
1:50

RevID	CHD	Revision	Date

CLIENT
PAIHIA TERRACES Ltd.

ADDRESS
**30-34 SELWYN ROAD,
PAIHIA
BAY OF ISLANDS
FAR NORTH DISTRICT COUNCIL**

PROJECT
DEVELOPMENT STAGE 1

CTM
C.T.M. ARCHITECTURAL Ltd.

CTM ARCHITECTURAL LTD
18 SWEETCORN PLACE,
PUNAKOHE, 2100
PO Box 547
PUNAKOHE, 2040
ph/fax: 09 238 4414
web: www.ctm.net.nz
e-mail: info@ctm.net.nz

WORK TO NZ B.C. AND RELEVANT CODES
DIMENSIONS SUPERSEDE SCALE
CHECK AND VERIFY ALL LEVELS AND DIMENSIONS
ON SITE BEFORE STARTING ANY WORK.

NOTE: ALL ELECTRONIC AND HARD COPY DOCUMENTS
INCLUDING INTELLECTUAL PROPERTY, ARE CONFIDENTIAL &
REMAIN THE PROPERTY OF "CTM ARCHITECTURAL LTD".

ISSUE
SCHEME 4 - PRELIM RC

DRAWING NAME
ELEVATIONS

DRAWN	SCALE	SHEET NUMBER	SHEET TOTAL
CRAIG	AS SHOWN		

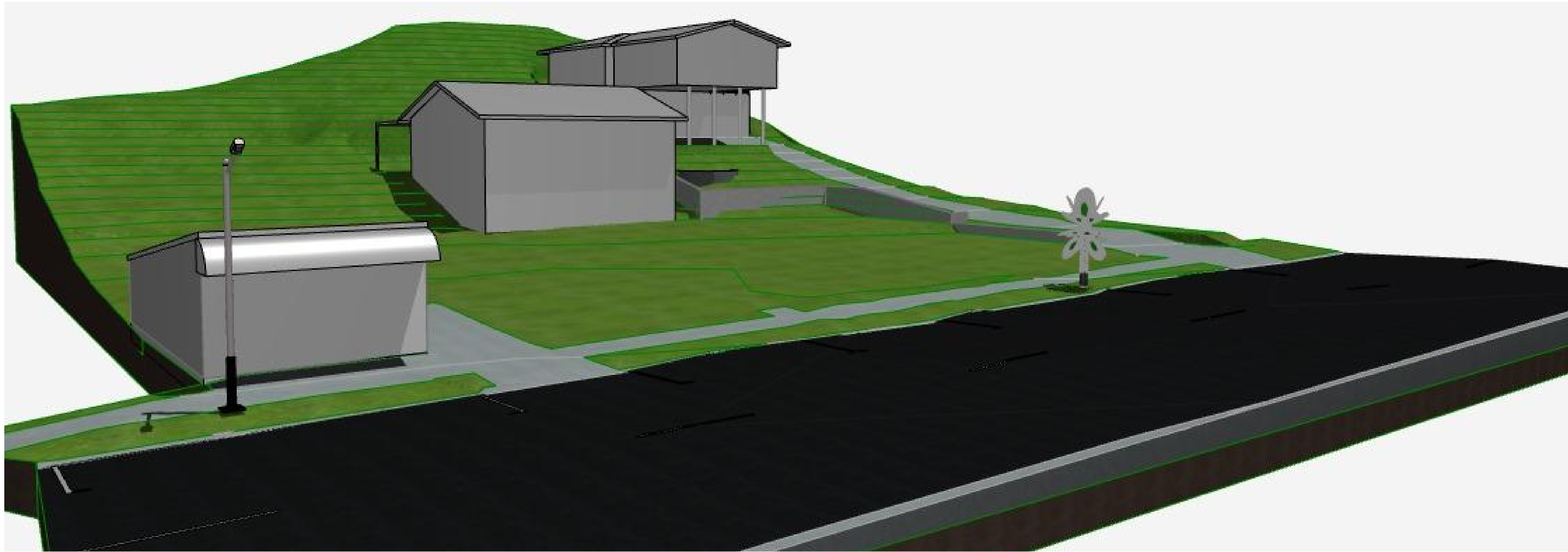
DATE
05 NOV 2024

JOB REF
24-059

TOTAL SHEETS
11

SHEET
11

REVISION



EXISTING



PROPOSED

RevID	CND	Revision	DATE
CLIENT			
PAIHIA TERRACES Ltd.			
ADDRESS			
30-34 SELWYN ROAD, PAIHIA BAY OF ISLANDS FAR NORTH DISTRICT COUNCIL			
PROJECT			
DEVELOPMENT STAGE 1			



CTM ARCHITECTURAL LTD
18 SWEETCORN PLACE,
PUNAKOHE, 2100
PO Box 547
PUNAKOHE, 2040
ph/fax: 09 238 4414
web: www.ctm.net.nz
e-mail: info@ctm.net.nz



WORK TO N.Z.B.C. AND RELEVANT CODES
DIMENSIONS SUPERSEDE SCALE
CHECK AND VERIFY ALL LEVELS AND DIMENSIONS
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ISSUE

SCHEME 4 - PRELIM RC

DRAWING NAME

VISUALS 1

DRAWN	SCALE	A08
CRAIG	AS SHOWN	
DATE	SHEET FORMAT	
05 NOV 2024	A1	
JOB REF	TOTAL SHEETS	
24-059	11	

SHEET REVISION



PROPOSED



PROPOSED



PROPOSED



PROPOSED



PROPOSED

RevID	CHD	Revision	Date
CLIENT PAIHIA TERRACES Ltd.			
ADDRESS 30-34 SELWYN ROAD, PAIHIA BAY OF ISLANDS FAR NORTH DISTRICT COUNCIL			
PROJECT DEVELOPMENT STAGE 1			

CTM ARCHITECTURAL LTD
18 SWEETCORN PLACE,
PUNEHONGE, 2120
PO Box 547
PUNEHONGE, 2040
ph/fax - 09 238 4414
web - www.ctm.net.nz
e-mail - info@ctm.net.nz

WORK TO N.Z.C. AND RELEVANT CODES
DIMENSIONS SUPERSEDE SCALE
CHECK AND VERIFY ALL LEVELS AND DIMENSIONS
ON SITE BEFORE STARTING ANY WORK.

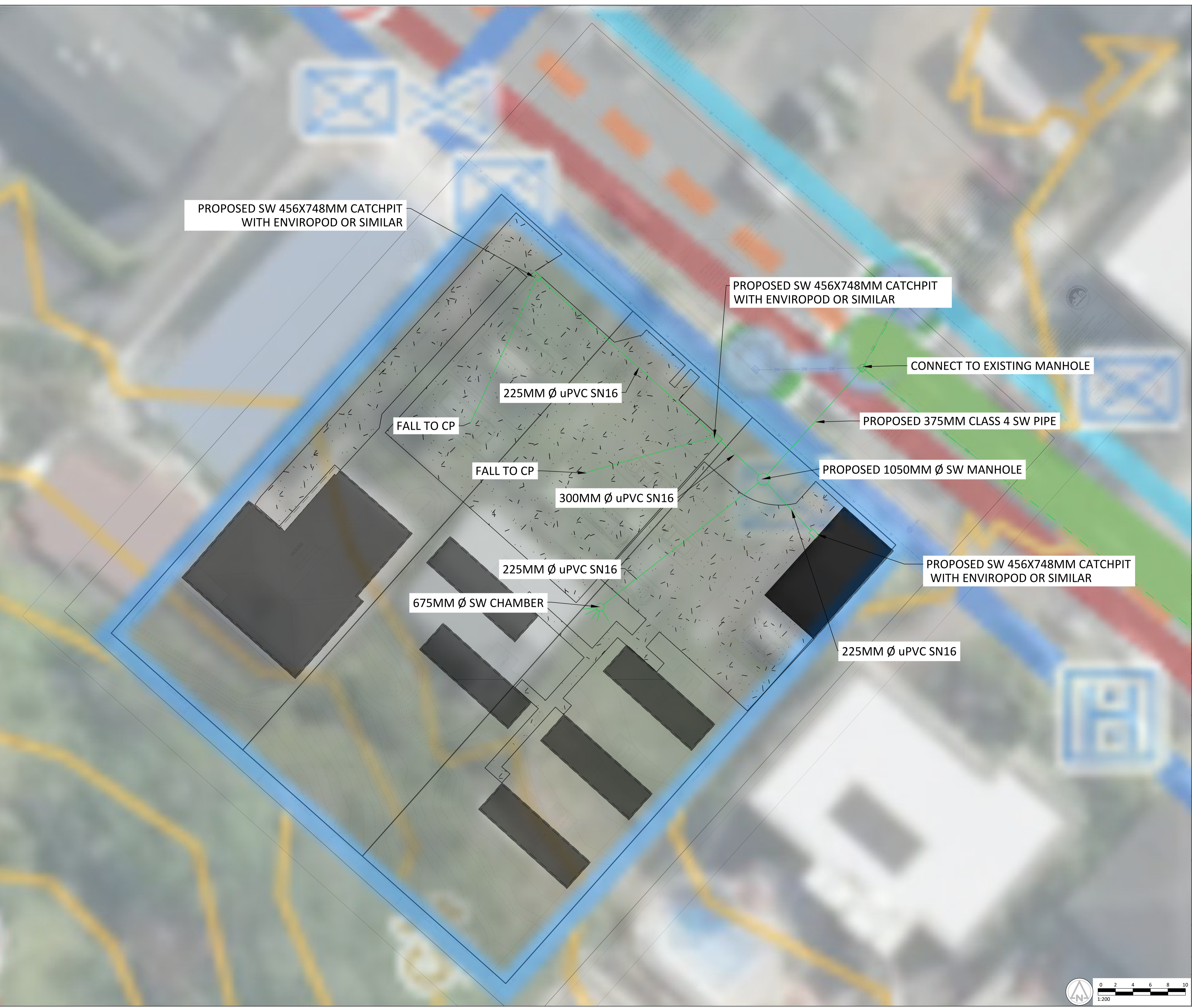
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ISSUE
SCHEME 4 - PRELIM RC

DRAWING NAME
VISUALS 3

DRAWN	SCALE	A10
CRAIG	AS SHOWN	
DATE	SHEET FORMAT	
05 NOV 2024	A1	SHEET
JOB REF	TOTAL SHEETS	
24-059	11	

APPENDIX B
GWE DRAWINGS



ISSUE STATUS: **FINAL**

REV	AMENDMENT	CAD	ENG	APPD	DATE	
0	FINAL		MY	TS	TS	02/12/24

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GWE CONSULTING ENGINEERS

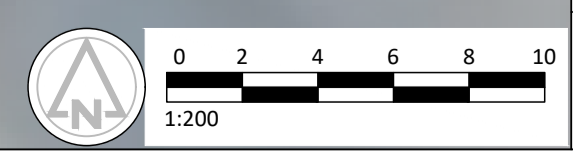
GWE CONSULTING ENGINEERS
 GRD FLOOR OCEANBRIDGE HOUSE
 25 ANZAC STREET TAKAPUNA
 AUCKLAND 0622
 P:+64 9 445 8338
 www.gwe.co.nz

PROJECT ADDRESS:
30, 32, 34 SELWYN ROAD, PAIHIA

DRAWING TITLE:
STORMWATER SITE PLAN

CLIENT NAME:
PAIHIA TERRACES LTD.

SCALE: 1:150		A1
PROJECT No: J2571-1	DRAWING No: 400	REV 0



GENERAL NOTES:

1. THE EXISTING SERVICES SHOWN ARE INDICATIVE ONLY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING, VERIFYING & PROTECTING ALL EXISTING UNDERGROUND, ABOVE GROUND SERVICES & OTHER FEATURES FOR THE DURATION OF THE WORKS. NOTIFY THE ENGINEER IMMEDIATELY SHOULD ANY DISCREPANCIES BE FOUND.
2. ALL WORKS SHALL BE UNDERTAKEN IN ACCORDANCE WITH AUCKLAND COUNCIL REQUIREMENTS, INCLUDING THE LATEST EDITIONS OF AUCKLAND COUNCIL, AUCKLAND TRANSPORT AND WATERCARE SERVICES CODES OF PRACTICE AND RELEVANT ENGINEERING STANDARDS. THE PROJECT SPECIFICATION AND ANY RESOUE CONSENT CONDITIONS RELATING TO THE SITE AND OTHER RELEVANT REQUIREMENTS.
3. LOCATION OF LEGAL BOUNDARIES SHOWN IN THE DRAWINGS IS ONLY INDICATIVE. SETOUT AND LOCATION OF LEGAL BOUNDARIES TO BE CONFIRMED BY CONTRACTOR BEFORE CARRYING OUT THE PHYSICAL WORKS. IF DURING SETTING OUT OF THE WORKS, A DISCREPANCY IS FOUND,THE CONTRACTOR SHALL NOTIFY THE ENGINEER AS SOON AS HE BECOMES AWARE OF THE DISCREPANCY AND PRIOR TO PROCEEDING WITH THE WORK. FAILURE IN DOING SO MAY RESULT IN THE CONTRACTOR UNDERTAKING ANY REMEDIAL WORKS AT HIS OWN COST.
4. THE WORKS SHALL BE UNDERTAKEN IN CONJUNCTION WITH ALL OTHER RELEVANT CONTAMINATED LAND WHICH MAY INCLUDE BUT NOT BE LIMITED TO GEOTECHNICAL ENGINEERING, LANDSCAPE ARCHITECTURE, TRAFFIC ENGINEERING, URBAN DESIGN AND STRUCTURAL ENGINEERING. LIAISE WITH THE DESIGN CONSULTANTS THROUGHOUT THE WORKS AS REQUIRED.
5. THE WORKS SHALL BE CONDUCTED IN STRICT ACCORDANCE WITH RELEVANT HOURS OF WORK AND NOISE LIMITS AS SET OUT IN THE

DISTRICT PLAN, OR CONSENT CONDITIONS, WHICHEVER IS MORE CONSERVATIVE.

6. THE CONTRACTOR SHALL LIAISE WITH THE UTILITY SERVICES AUTHORITY FOR THE RE-LOCATION AND/OR REMOVAL OF ANY EXISTING TELECOM AND POWER POSTS WITHIN THE SITE BOUNDARY.
7. ALL WORKS ON COMMUNICATIONS, POWER AND GAS SERVICES ARE TO BE CARRIED OUT ONLY BY APPROVED UTILITY SERVICES CONTRACTORS/TRADESMEN. ANY EXCAVATION AND REINSTATEMENT OF TRENCHES WITHIN PUBLIC LAND SHALL BE UNDERTAKEN IN ACCORDANCE WITH THE REQUIREMENTS OF AUCKLAND COUNCIL CODE OF PRACTICE FOR EXCAVATION AND REINSTATEMENT OF TRENCHES.
8. REFER TO THE SPECIFICATIONS FOR THE CONSTRUCTION OF STORMWATER DRAINAGE WORKS. SHOULD A CONFLICT EXIST BETWEEN THE SPECIFICATION, AUCKLAND COUNCIL (OR WATERCARE SERVICES LIMITED) CODE OF PRACTICE, THE HIGHER OF THE TWO SHALL APPLY.
9. ALL WORKS ON WATERCARE ASSETS SHALL BE CARRIED OUT BY WATERCARE SERVICES LIMITED APPROVED CONTRACTORS ONLY.
10. ALL DRAINAGE PIPEWORK SHALL BE CONSTRUCTED BY WAY OF OPEN TRENCH INSTALLATION UNLESS OTHERWISE NOTED.
11. WHERE MINIMUM COVERS IN ACCORDANCE WITH AUCKLAND COUNCIL'S CODE OF PRACTICE CANNOT BE ACHIEVED, SPECIFIC DESIGN OF THE PIPELINE OR PROTECTION SHALL BE UNDERTAKEN TO THE APPROVAL OF AUCKLAND COUNCIL.
12. THE MINIMUM CLEANCE BETWEEN STORMWATER AND WASTEWATER DRAINAGE LINES AND OTHER SERVICES SHALL BE 150mm.
13. ALL SADDLE CONNECTIONS SHALL BE PROVIDED WITH APPROVED FLEXIBLE JOINTS.
14. ALL PIPE TRENCHES UNDER ROAD, PARKING OR DRIVEWAY AREAS SHALL BE BACKFILLED WITH SUITABLE, WELL GRADED GAP65 HARDFILL AS APPROVED BY THE ENGINEER.

CUT / FILL NOTES

1. CUT TO FILL VOLUMES ARE FROM EXISTING GROUND INCLUDING TOP SOIL TO FINAL GROUND INCLUDING TOPSOIL, PAVEMENT AND TO FINISHED FLOOR LEVEL.
2. NO BULKING FACTORS HAVE BEEN USED IN THE VOLUME ESTIMATION.

PUBLIC STORMWATER NOTE:

1. ALL STANDARD STORMWATER MANHOLES 1050mm DIAMETER TO BE HD WITH HD LIDS. MINI MANHOLES AND INSPECTION CHAMBERS SHALL BE 675mm DIAMETER.
2. ALL NEW SWMH'S ARE TO BE CONSTRUCTED AS PER ACSD SW05 AND WSL SD WW8.
3. USE SLIDING JOINT WHEN CONNECTING PE PIPE WITH GRADIENTS LESS THAN 7% TO MANHOLES AS PER WSL SD WW11.
4. USE RESTRAINED JOINT WHEN CONNECTING PE PIPE WITH GRADIENTS 7% AND STEEPER TO MANHOLES AS PER WSL SD WW12.
5. USE LOW-STRENGTH CONCRETE BEDDING FOR RC AND PVC PIPES ON GRADIENTS BETWEEN 10% AND 20% AS PER ACSD SW23.
6. USE ANCHOR BLOCKS FOR RC AND PVC PIPES ON GRADIENTS STEEPER THAN 20% AS PER ACSD SW23.

SEDIMENT AND EROSION CONTROL NOTES:

1. THESE PLANS DETAIL THE GENERAL SEDIMENT AND EROSION CONTROL MEASURES. ACTUAL CONTROLS ARE TO BE THE RESPONSIBILITY OF THE CONTRACTOR AND ARE TO BE ADAPTED TO SUIT THE CURRENT STAGE OF WORKS.
2. ALL WORKS ARE TO BE IN ACCORDANCE WITH AUCKLAND COUNCIL GUIDANCE DOCUMENT 2016/05 (GD05), EROSION AND SEDIMENT CONTROL GUIDE.

EARTHWORK NOTES:

1. FINISHED GROUND MODEL TO TOP OF PAVEMENT FOR ALL HARDSTAND AREAS, FINISHED GROUND LEVEL OF BUILDINGS AND TOP OF TOPSOIL IN ALL OTHER AREAS.
2. TEMPORARY EARTHWORKS, SHORING, AND ENABLING WORKS TO BE DESIGNED BY OTHERS AND ARE THE RESPONSIBILITY OF THE CONTRACTOR.

GENERAL ABBREVIATIONS		UTILITY ABBREVIATIONS	
EX	EXISTING	SW	STORMWATER
PROP	PROPOSED	WW	WASTEWATER
BNDY	BOUNDARY	WS	WATER SUPPLY
RL	REDUCED LEVEL	PUB	PUBLIC
FFL	FINISH FLOOR LEVEL	PVT	PRIVATE
PFL	PLATFORM LEVEL	MH	MANHOLE
RW	RETAINING WALL	IP	INSPECTION POINT
TOW	TOP OF WALL	CP	CATCH PIT
BOW	BOTTOM OF WALL	SP	SPLAY PIT
DP	STORMWATER DOWN PIPE	LL	LID LEVEL
GT	WASTEWATER GULLY TRAP	IL	INVERT LEVEL
GEOMETRY ABBREVIATIONS		RCRRJ	REINFORCED CONCRETE RUBBER RING JOINT
L	LEFT	PE	POLYETHYLENE
R	RIGHT	uPVC	UNPLASTICIZED POLYVINYL CHLORIDE
CL	CENTRE LINE	AC	ASBESTOS CONCRETE
HP	HIGH POINT	VC	VITRIFIED CLAY
LP	LOW POINT	EW	EARTHENWARE
CH	CHAINAGE	CONC	CONCRETE
BOA	BEGIN OF ALIGNMENT	CLS	CEMENT LINED STEEL
EOA	END OF ALIGNMENT	DI	DUCTILE IRON
BP	BEGIN POINT	WS	WATER SERVICE
EP	END POINT	SV	SLUICE VALVE
MID	MIDDLE POINT	GV	GATE VALVE
PC	POINT OF CURVATURE	FH	FIRE HYDRANT
PCC	POINT OF COMPOUND CURVATURE	EC	END CAP
PRC	POINT OF REVERSE CURVATURE	F	FIBRE
PT	POINT OF TANGENCY	V	ISOLATION VALVE
IP	INTERSECTION POINT	E	ELECTRICAL POWER
BLS	BEGIN LONGSECTION	OH	OVERHEAD POWER
ELS	END LONGSECTION	UN	UNDERGROUND POWER
VPC	VERTICAL POINT OF CURVATURE	G	NATURAL GAS
VPT	VERTICAL POINT OF TANGENCY	T	TELECOMMUNICATIONS
BRK	GRADE BREAK	CS	COMBINED SERVICES
K	CURVE COEFFICIENT		

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AUCKLAND 0622
P:+64 9 445 8338
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PROJECT TITLE:	STORMWATER MANAGEMENT	PROJECT No :	J2571-1
PROJECT ADDRESS:	30, 32, 34 SELWYN ROAD, PAIHIA	DRAWING No :	401
CLIENT:	PAIHIA TERRACES LTD.	REV	0
DRAWING TITLE:	STORMWATER NOTES	SCALE:	N/A

0	FINAL	MY	TS	TS	02/12/24
REV	AMENDMENT	CAD	ENG	APPD	DATE
ISSUE STATUS:		FINAL			

APPENDIX C
SUPPORTING CALCULATIONS

30, 32, 34 Selwyn Rd, Paihia



Stormwater Pipe Flow & Capacity Assessment	Reference		Revision	1
	Prepared by	MY	Date	12-Dec-2024
	Checked by		Date	

Peak Flow 10 year ARI 10-min + Climate Change

Existing Site Impervious Areas

Description	Area (ha)	Runoff Coefficient (c)	Intensity (mm/hr)	Peak Flow, $Q = 2.78CiA$ (L/s)
Impervious	0.04504	0.9	140.25	15.8
Pervious	0.18296	0.3	140.25	21.4
Total Area	0.22800		Total Flow (L/s)	37.2

(Areas measured from CTM Architecture plans)

Proposed Site Impervious Areas

Description	Area (ha)	Runoff Coefficient (c)	Intensity (mm/hr)	Peak Flow, $Q = 2.78CiA$ (L/s)
Impervious	0.11374	0.9	140.25	39.9
Pervious Area	0.11426	0.3	140.25	13.4
Total Area	0.22800		Total Flow (L/s)	53.3

(Areas measured from architect's drawing)

Difference between pre and post development flow (L/s) 16.1

Catchment of SW Pipe (GIS ID: 3762), excluding the subject site

Description	Area (ha)	Runoff Coefficient (c)	Intensity (mm/hr)	Peak Flow, $Q = 2.78CiA$ (L/s)
Impervious Area	1.42520	0.9	140.25	500.1
Pervious	0.95820	0.3	140.25	112.1
Total Area	2.3834		Total Flow (L/s)	612.2

(Areas measured from Far North Maps Aerial Photo)

Pipe Capacity Assessment (Colebrook-White Formula)

Roughness factor(k): k=0.6 for pipe diameter > 1000mm
k=1.5 for pipe diameter < 1000mm

Pipe diameter (mm)	Grade (%)	Velocity (m/s)	Capacity (L/s)	Post-development 10-Yr ARI Peak Flow
600	1.25	2.43	685.8	665.5

Public Stormwater Pipe, Concrete (GIS ID: 2000454092)

Traffic Assessment

This development proposal includes 5 tourist cabins, 5 overnight parking spaces for self-contained campervans, and 7 all-day parking spots. The expected vehicle trips have been calculated based on standard trip generation rates widely used in New Zealand and supported by transport-related research reports. The following analysis explains the logic behind the estimated vehicle trips and demonstrates how the traffic generated will remain well within acceptable limits for the area.

1. Tourist Cabins

The 5 tourist cabins will generate an estimated 8 vehicle trips per cabin per day. This figure is derived from typical trip generation data for tourist accommodations, which accounts for guests arriving, departing, and making additional trips during their stay for local activities such as dining or sightseeing. According to Research Report 453 from the New Zealand Transport Agency (NZTA), accommodations like motels or tourist cabins generate between 6 to 10 vehicle trips per unit per day depending on the nature of the accommodation. Given the location of the cabins and the walking accessibility to town we have gone with the lower number. But even at the higher number it still puts us well within the permitted activity for the zoning.

Total trips from cabins: 5 cabins × 6 trips = 30 vehicle trips per day.

2. Overnight Parking Spaces for Self-Contained Campervans

The 5 parking spots for self-contained campervans are expected to generate approximately 4 vehicle trips per spot per day. These trips include the arrival and departure of the campervans and allowing for 2 vehicles per spot per day. Data suggests that campervans generally generate fewer trips than other forms of tourist accommodation, given that they often remain parked overnight without frequent vehicle movements. The nature of these parks are that once a campervan is there, they will be staying for at least 12-24 hours. Those campervans looking for short term car parking will use the FNHL carpark or street car parks nearby.

Total trips from campervans: 5 spaces × 4 trips = 20 vehicle trips per day.

3. All-Day Parking Spaces

The 7 all-day parking spaces will be used by visitors arriving in the morning and departing in the evening, generating 2 vehicle trips per day per space. This is based on the assumption that these spaces will serve visitors who are looking for all day parking options in close proximity to the Paihia wharf. These are typically fishermen heading out at 8am and returning around 5pm. Or families heading out on cruises throughout the Bay. These are designed as single price all day parking and cars here will not be making multiple trips during their stay.

Total trips from all-day parking: 7 spaces × 2 trips = 14 vehicle trips per day.

Total Estimated Traffic Movements

By adding the expected vehicle trips from each component of the development, we estimate a total of 64 vehicle trips per day:

Tourist cabins: 30 trips

Campervan spaces: 20 trips

All-day parking spaces: 14 trips

Total daily vehicle trips: $30 + 20 + 14 = 64$ vehicle trips per day.
This represents 31% of the total trips allowed as a permitted activity, therefore we have plenty of room for a margin of error.

Conclusion

The estimated total of 64 vehicle trips per day is consistent with traffic generated by similar small-scale tourist accommodations in New Zealand. These estimates are in line with standards used in traffic impact assessments and comply with the Far North District Council's guidelines for traffic intensity thresholds. Given the commercial nature of the area and the anticipated low volume of additional traffic, this development is not expected to place undue strain on the local road network or require significant traffic mitigation measures.

The calculations and logic presented above are supported by established traffic generation data sources, including NZTA Research Report 453, which provides trip generation data specifically for tourist accommodation developments. Furthermore, the trip generation thresholds established by Far North District Council ensure that developments of this nature can be managed without the need for extensive traffic management interventions.

Author: Kyron Gosse

This traffic assessment was contained in an email sent to Ishan Koshatwar and Yuna Zhou FNDC dated 10 October 2024.

These References were attached to the email dated 10 October 2024:

NZTA Research Report 453: <https://www.nzta.govt.nz/resources/research/reports/453/>

Far North District Council District Plan:

Far North Maps - Zoning and traffic considerations

District Plan Review – Trip Threshold in TRAN Table 11

Front Left		
A5	Sabal minor	Dwalf Palmetto
A7	Syagrus romanzoffiana	Queen Palm, Cocos Palm
C1	Agave attenuata	Agave
C2	Anemanthele lessoniana	Gossamer Grass, New Zealand Wind Grass, Pheasant's Tail Grass, Hunangamoho
D2	Coprosma	Coprosma
D3	Grevillea	Grevillea Groundcovers

Front Right		
A6	Strelitzia reginae	Bird of Paradise Flower, Crane Flower, Strelitzia
A7	Syagrus romanzoffiana	Queen Palm, Cocos Palm
C1	Agave attenuata	Agave
C16	Lomandra longifolia c.'Lime Tuff'	Mat Rush
D4	Pratia	

Cabins		
A5	Sabal minor	Dwalf Palmetto
A7	Syagrus romanzoffiana	Queen Palm, Cocos Palm
C2	Anemanthele lessoniana	Gossamer Grass, New Zealand Wind Grass, Pheasant's Tail Grass, Hunangamoho
C3	Apodasmia similis	OiOi, Jointed Wire Rush, Segmented Rush, Thatching Rush
C6	Carissa macrocarpa	Natal Plum
C7	Cassia fistula	Indian Laburmum, Golden Shower Tree, Pudding-Pipe, Monkey-Pod Tree
C9	Cytisus proliferus (Chamaecytisus proliferus var palmensis)	Tagasaste, Tree Lucern
C11	Dierama pulcherrimum	Angel's Fishing Rod, Wand Flower
C12	Grevillea dimorpha	Flame Grevillea

C18	Plumeria obtusa	White Frangipani
C19	Plumeria rubra	Red Frangipani
C20	Senna (Cassia) alata	Candle Bush
C21	Senna (Cassia) artemisioides	Silver Cassia, Puntty
C27	Tibouchina lepidota	Alstonville, [Lasiandra, Glory Bush]
C29	Trachelospermum jasminoides	Star Jasmine, Confederate Jasmine, Chinese Star Jasmine, Chinese Ivy, Trader's Compass
C31	Pittosporum eugenioides	Lemonwood, Tarata
D2	Coprosma	Coprosma
D3	Grevillea	Grevillea Groundcovers
D4	Pratia	